

# Chapter 4:

## Heritage & Cultural Conservation



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## Document Control for Drafting Only

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*Title Page: The picture on the title page is of the painted silos by the artist Guido Van Helten at the State Heritage Listed ‘The Foundations Site’, Portland (Source: [www.thefoundations.com.au](http://www.thefoundations.com.au) (Instagram - <https://www.instagram.com/p/BxwXzQbHqTe/>)).*

## 4.1. Introduction

### 4.1.1. Application of this Chapter

This chapter of the DCP applies to development on land that is:

- a) Identified as a **heritage item** or located in a **heritage conservation area** listed in *Schedule 5* of the **Lithgow Local Environmental Plan 2014 (LLEP2014)** or in the [State Heritage Register](#);
- b) In proximity to an identified **heritage item** that may impact on the heritage significance of a **heritage item**;
- c) Identified as, or in the vicinity to, other locally or regionally significant buildings or places that are not listed in Schedule 5 of the [LLEP2014](#) or identified in a Section 170 Heritage and Conservation Register at the discretion of Council staff; or
- d) Identified as, or in vicinity to, items of **Aboriginal cultural and archaeological significance** that are not listed in [LLEP2014](#) for protection and cultural sensitivity.

*Refer to Councils Heritage Conservation & Management [website](#) and **DA Guide** for further information regarding Heritage Conservation within the Lithgow LGA. If you require further information, contact Councils Economic Development and Environment Department.*

### 4.1.2. Objectives of Heritage Conservation

- 1) To conserve the heritage significance of the built and natural environments of the Lithgow Local Government Area and ensure new development is sympathetic to the identified heritage values.
- 2) To ensure the management of heritage is carried out in line with the Principles and Articles of the [Burra Charter](#) for the assessment of significance of heritage places and the traditions associated with them.
- 3) To implement the objectives and controls contained within [LLEP2014](#) and any other relevant heritage legislation.
- 4) To provide planning and design guidelines for developments associated with **heritage items** or a **heritage conservation area**.

*Refer to the following publications as a guide to heritage conservation and what the best practice principles are to undertaking heritage conservation:*

- [A Guide to the Heritage System](#)
- [The Burra Charter](#)
- [Heritage Listing Explained](#)
- [Planning and Heritage](#)
- [Levels of Significance](#)

### 4.1.3. Other Relevant Chapters of this DCP

Please remember that this Chapter of the DCP is unlikely to contain ALL of the relevant controls for your development.

Please see DCP *Chapter 1 – Introduction & Administration* to review the Section on *How to Use this DCP* including the *Structure of the DCP* (see table below) to determine what other Chapters may be relevant to your development.

**IF YOU ARE UNSURE, PLEASE DISCUSS THIS WITH COUNCIL STAFF PRIOR TO LODGING YOUR APPLICATION.**

*The DCP has the following Chapters:*

<b>Chapter 1:</b>	Introduction & Administration
<b>Chapter 2:</b>	Site Requirements
<b>Chapter 3:</b>	Natural Environment & Hazards
<b>Chapter 4:</b>	Heritage & Cultural Conservation <b>(THIS CHAPTER)</b>
<b>Chapter 5:</b>	Subdivision & Roads
<b>Chapter 6:</b>	Residential Development
<b>Chapter 7</b>	Commercial, Community & Industrial Development (including Advertising/ Signage for all relevant land uses)
<b>Chapter 8:</b>	Rural & Other Land Uses
<b>Chapter 9:</b>	Pottery Estate Precinct

### 4.1.4. Exempt & Complying Development

Please note that [State Environment Planning Policy \(Exempt and Complying Development Codes\) 2008 \(Codes SEPP\)](#) may permit certain development set out in that policy without requiring a development application to Council if it complies with the requirements of the **Codes SEPP**. Please discuss this with Council or visit the [NSW Government Planning Portal](#).

## 4.1.5. Definitions

TERM	DEFINITION
<b>Aboriginal object</b>	means any deposit, object or other material evidence (not being a handcraft made for sale) relating to the Aboriginal habitation of an area of New South Wales, being habitation before or concurrent with (or both) the occupation of that area by persons of non-Aboriginal extraction and includes Aboriginal remains.
<b>Aboriginal place of heritage significance</b>	means an area of land, the general location of which is identified in an Aboriginal heritage study adopted by the Council after public exhibition and that may be shown on the <i>Heritage Map</i> , that is: <ul style="list-style-type: none"> <li>a) the site of one or more Aboriginal objects or a place that has the physical remains of pre-European occupation by, or is of contemporary significance to, the Aboriginal people. It may (but need not) include items and remnants of the occupation of the land by Aboriginal people, such as burial places, engraving sites, rock art, midden deposits, scarred and sacred trees and sharpening grooves, or</li> <li>b) a natural Aboriginal sacred site or other sacred feature. It includes natural features such as creeks or mountains of long-standing cultural significance, as well as initiation, ceremonial or story places or areas of more contemporary cultural significance.</li> </ul>
<b>Adaptive reuse of buildings</b>	is a process that changes a place that is no longer suitable or required for its original purpose to a place that can be used for a new purpose.
<b>Aboriginal Heritage Information Management System (AHIMS)</b>	contains information and records about Aboriginal objects that have been reported to the Director General of the Department of Premier and Cabinet. It also contains information about Aboriginal Places which have been declared by the Minister for the Environment to have special significance with respect to Aboriginal culture. <b>AHIMS</b> refers to these recorded Aboriginal objects and declared Aboriginal Places as 'Aboriginal sites'.
<b>Archaeological site</b>	means a place that contains one or more relics. This evidence may include objects and artefacts of everyday life such as crockery, bottles, tools and the remains of early buildings and structures.
<b>Curtilage</b>	in relation to a <b>heritage item</b> or <b>conservation area</b> , means the area of land (including land covered by water) surrounding a heritage item, a <b>heritage conservation area</b> , or building, work or place within a <b>heritage conservation area</b> , that contributes to its heritage significance.
<b>Heritage item</b>	means a building, work, place, relic, tree, object or archaeological site the location and nature of which is described in Schedule 5 of <a href="#">LLEP2014</a>
<b>Heritage conservation area</b>	means an area of land that has significant streetscape character and are of value due to the collective nature of buildings and elements in that area. The location and nature are described in Schedule 5, Chapter 2 of <a href="#">LLEP2014</a> .

TERM	DEFINITION
<b>Heritage conservation management plan (HCMP)</b>	means a document prepared in accordance with guidelines prepared by the Division of the Government Service responsible to the Minister administering the <i>NSW <a href="#">Heritage Act 1977</a></i> that documents the heritage significance of an item, place or heritage conservation area and identifies conservation policies and management mechanisms that are appropriate to enable that significance to be retained.
<b>Heritage impact statement (HIS)</b>	means a document consisting of: <ul style="list-style-type: none"> <li>a) a statement demonstrating the heritage significance of a heritage item or heritage conservation area, and</li> <li>b) an assessment of the impact that proposed development will have on that significance, and</li> <li>c) proposals for measures to minimise that impact.</li> </ul>
<b>Heritage management document (HMD)</b>	means: <ul style="list-style-type: none"> <li>a) a <b>heritage conservation management plan</b>, or</li> <li>b) a <b>heritage impact statement</b>, or</li> <li>c) any other document that provides guidelines for the ongoing management and conservation of a heritage item, Aboriginal object, Aboriginal place of heritage significance or heritage conservation area.</li> </ul>
<b>Local significance</b>	items of local heritage significance result from previous Heritage Studies, the National Trust listings, the Lithgow Shire Community Based Heritage Study and local community groups and are identified in Schedule 5, Chapter 1 of <b>LLEP2014</b> .
<b>National significance</b>	items of national heritage significance are items having significance beyond the State level and are considered under the Australian <i>Heritage Council Act 2003</i> . The Act affects property owned by the Commonwealth Government and its Agencies.
<b>Outbuildings</b>	means an ancillary building that does not include an attached garage or carport.
<b>Relic</b>	is defined in the Heritage Act as any artefact, object or material evidence which relates to the settlement of the area that comprises New South Wales, not being Aboriginal settlement, and which is of State or local heritage significance.
<b>State significance</b>	state significant items are identified in the <a href="#">State Heritage Register</a> and Heritage Office under the NSW <a href="#">Heritage Act 1977</a> .
<b>Sympathetic development</b>	means undertaking changes to an item or place whilst maintaining its heritage significance. This may require retaining the form and architectural style of an item, such as retaining original roof pitch, materials, window placement and scale, setbacks and colour treatment or placing extensions at the rear of the building, which all play a role in maintaining and conserving the character of an item or place.

## 4.2. Development Consent Requirements

To undertake changes to a heritage listed item or an item in close vicinity to a heritage listed item, consent will be needed to be sought from Council before undertaking development. To obtain written consent from Council, a Development Application will need to be lodged barring specific exemptions in the LLEP2014. Through this process, Council can assess any potential impacts to a heritage listed item to ensure that any potential impacts to the structural integrity or values of a heritage item are understood and effectively mitigated. Due to the variation of heritage items and heritage conservation areas within the Lithgow LGA, each development proposal will have its own set of unique circumstances to consider and address.

### 4.2.1. Development Requiring Consent

A **Development Application** is required for the carrying out of most development which relates to a **heritage item, Aboriginal place of heritage significance or archaeological site**, and/or in a **Heritage Conservation Area** unless described in *DCP Section 4.2.2*.

[LLEP2014](#), Clause 5.10 (2) identifies the instances where development consent is required or alternatively contact Council's Economic Development and Environment Department.

### 4.2.2. Development Not Requiring Consent

There are circumstances where development consent is not required for certain works, such as:

- 1) [LLEP2014](#) Clause 5.10(3) contains some exemptions where development consent is not required. Otherwise, applicants need to notify Council prior to undertaking any maintenance or repair work to determine whether development consent is required. A written response is required from Council prior to the commencement of works.

*Council encourages the maintenance and upkeep of heritage items and properties in heritage conservation areas if they are of a minor nature or would not adversely affect the heritage values of an item or place.*

*Maintenance and repairs can include non-structural external works such as:*

- a) *Replacing broken windows, fly screens etc*
- b) *Minor repairs to roofing, brickwork, timberwork and metal work*
- c) *Repainting surfaces which are already painted (Council may be able to assist with suggesting sympathetic colour schemes) including timberwork and metalwork.*
- d) *Maintenance and repairs can also include non-structural internal works such as:*
- e) *Patching, painting and decoration to the interior to the house and installation of joinery items*
- f) *Repairing timber floors*
- g) *Plumbing and gas fitting work*



h) *Electrical work and communications cabling*

i) *Installation of insulation*

*If unsure contact Council's Economic Development and Environmental Department.*

### 4.2.3. Development Application Requirements

When lodging a **Development Application** for development on land on which a **heritage item** is situated, or within a **heritage conservation area**, or within the vicinity of a **heritage item** or **heritage conservation area**, Council may require a **Heritage Management Document (HMD)** to be prepared and submitted for assessment.

To assist in understanding the background of a heritage item, individual Heritage Inventory Sheets have been prepared for each heritage item. These provide basic background information about the history of the property, a description of the item, and an assessment of the various heritage values. These heritage inventory sheets should be referred to early in the development process. They are also required to be considered when preparing a heritage management document for submission to Council with a development application. The inventory sheets are available from Council and are also on the [NSW Office of Environment and Heritage \(OEH\) Website](#).

*The circumstances in which a **HMD** may be required will vary depending on the works proposed and the likely impact they will have on the heritage significance of the item or HCA. Discussions with Council staff and the heritage advisor are recommended to determine what may be needed to assist in assessing a **DEVELOPMENT APPLICATION**.*

The HMD could include:

- 1) **Heritage Impact Statement (HIS)** which considers the extent to which a proposal would affect the heritage significance of a heritage item of heritage conservation areas.
  - a) A **HIS** establishes the heritage significance of a place, assesses the impact of the development on this significance, and proposes measures to minimise impact.
  - b) A **HIS** is generally required for development of a **heritage item** or property within or in close proximity to a **Heritage Conservation Area (HCA)**, including alterations and additions, demolition or construction, or a replacement building.
  - c) For major changes or demolition, a **HIS** should generally be developed by a heritage consultant; OR
- 2) **Conservation Management Plan (CMP)** which develops conservation policies for the heritage item and management frameworks and mechanisms to enable heritage significance to be

retained. The conservation policy should explain the principles to be followed or retain or reveal an item's significance which demonstrates how the significance of the item can be enhanced and maintained.

- a) It is particularly useful where building fabric has deteriorated, and to facilitate master planning and asset management for a large site or area or the item is listed on the [State Heritage Register](#).
- b) Council may ask for a **CMP** before granting consent under Clause 5.10(6) of [LLEP2014](#)
- c) A **CMP** should be prepared by a heritage consultant.

*Refer to the following publications as a guide to developing Heritage Management Documents:*

[Conservation Management Documents](#)

[Conservation Plan](#)

[Statements of Heritage Impact](#)

#### 4.2.4. Archaeological Sites (Non-Indigenous)

- 1) All known and potential archaeological relics in NSW are protected under the NSW [Heritage Act 1977](#) (as amended). When intending to disturb or excavate land where archaeological relics have been identified or are considered likely to occur, it is the responsibility of the property owner to seek relevant approvals.
- 2) Before granting consent to any development on land on which an archaeological site is identified, Council notifies the NSW Heritage Council of its intention to grant consent and take into consideration any response from the NSW Heritage Council.
- 3) If relics are discovered during construction or operation, works should cease immediately and contact the Heritage Office. An assessment from an archaeologist may be required prior to construction or operation resuming.

#### 4.2.5. Aboriginal Places of Heritage Significance

Impact on an Aboriginal place of heritage significance or an Aboriginal object is considered by Council before granting consent. Specifically, *Clause 5.10(8)* of [LLEP2014](#) states that Council:

- 1) Consider the effect of the proposed development on the heritage significance of the place and any Aboriginal object known or reasonably likely to be located at the place; and
- 2) Notify the local Aboriginal communities (in such a way as Council thinks appropriate) about the application; and
- 3) Take into consideration any response received within 28 days after the notice is sent.

If an application is located along a significant watercourse or ridgeline or a likely Aboriginal place of heritage significance then please contact Council to discuss.

You may be required to conduct a search of the **Aboriginal Heritage Information Management System (AHIMS)** to see if an Aboriginal object or place has been declared on a parcel of land (though this search is not determinative of the presence of Aboriginal items).

If any Aboriginal object is found during the development process then work stops immediately, and the requirements of the [National Parks and Wildlife Act 1974](#) are followed. An applicant may also need to seek an **Aboriginal Heritage Impact Permit** in accordance with that Act.

#### 4.2.6. State Heritage Items

*Heritage items listed as being of State significance in Schedule 5 of the [LLEP2014](#) are those listed on the State Heritage Register.*

- 1) Any works to a [State Heritage Register](#) listed item (including demolition) requires approval or exemption under Clause 57 of the [Heritage Act 1977](#).
- 2) When a **Development Application** is lodged with Council for demolition or development of any type for a [State Heritage Register](#) listed item, the integrated development application process commences and Council will, as chapter of that process, refer the application to the Heritage Council for approval.

#### 4.2.7. Conservation Incentives

*Council recognises the need to be flexible with heritage items in terms of providing for their long-term conservation. [LLEP2014](#) Clause 5.10(10) - Conservation Incentives enables Council to consider approval of development relating to a heritage item or the site of a heritage item, which would otherwise be prohibited in the zone. Council offers a free Heritage Advisor service owners of heritage properties to assist in promoting positive urban design and heritage in the Lithgow LGA. Works may be able to seek heritage funding. Please contact Councils Economic Development and Environment Department.*

If an applicant seeks approval for development under [LLEP2014](#) Clause 5.10(10) - Conservation Incentives, Council must be satisfied that:

- a) The development is undertaken in accordance with an approved **HMD** (if required); and
- b) The applicant ensures that the necessary conservation work will be carried out in conjunction with the development.

### 4.2.8. Demolition

While the concept of heritage conservation is focused on preserving items which inform historical narratives, at times there may be unique circumstances where the preservation of a heritage item is not possible. Applicants should explore all avenues of retaining heritage items prior to submitting an application for demolishment.

- 1) All demolition work requires development consent. In assessing a DA for the demolition of a heritage item or a contributory building, Council will consider:
  - a) The heritage significance of the item or building;
  - b) The structural condition;
  - c) Comparative analysis of all options; and
  - d) The contribution the item or building makes to the **streetscape character**.
- 2) Buildings that are listed as **heritage items** or contribute to the significance of a **heritage item** are not to be (wholly or partly) demolished unless there are overriding reasons such as extensive structural damage. Council may require the submission of a report by a structural engineer with heritage experience as justification for demolition works.
- 3) Partial demolition of a **heritage item** should only be considered when it can be established in a **HMD** that the partial demolition will not have a substantial impact on the significance of the **heritage item**.
- 4) Outbuildings associated with **heritage items** can only be demolished where a **HMD** has established that the outbuilding does not contribute to the heritage significance of the place and development consent has been obtained.
- 5) Photographic records of building/s are to be provided to Council prior to the commencement of any demolition works. This is to be done in accordance with the [‘Photographic Recording of Heritage Items Using Film or Digital Capture’](#) (as amended) and [‘How To Prepare Archival Records of Heritage Items’](#).
- 6) Applications for demolition of a **heritage item** or buildings in a **heritage conservation area** are required to provide details on the replacement development.

### 4.2.9. Subdivision

Subdivision of an allotment that includes a **heritage item** or recognised **archaeological site** is not permitted unless:

- 1) It can be demonstrated that the **heritage item** will retain an 'appropriate' **curtilage** (buffer) – the size of which will depend on the significance and content of the **heritage item** and its relationship to the surrounding buildings and landscape.

- 2) It can be established that proposed or future development on the created allotments will not impact on important views to or from the **heritage item**.

*To determine the heritage curtilage of an item or important views to or from a heritage item, please consult with Council's Department of Economic Development and Environment. Refer to the Heritage Office's publication [Heritage Curtilage](#) for further information on defines a heritage curtilage.*

#### **4.2.10. Adaptive Reuse**

- 1) Adaptive reuse of a building should retain important architectural qualities and features of the building, where possible, particularly features that contribute to the streetscape and the heritage significance of the place.
- 2) Adaptive reuse involves only minimal change to the significant fabric of the place and acknowledges and respects significant associations and meanings of the place.
- 3) Adaptive reuse has regard to the relevant legislation of the [National Construction Code](#) (NCC) and other relevant **Australian Standards**.

## 4.3. General Controls - Development of Heritage Items/Places & within Heritage Conservation Areas

This section of the DCP contains the general objectives and controls that apply to development on and in the vicinity of heritage items and heritage conservation areas identified in the Lithgow LEP 2014. Due to the complex nature and variation of heritage in the Lithgow LGA, development proposals are to be considered and assessed by individual merit, taking into consideration the unique elements of the item and the issues of pertaining to the item's heritage values. Responses to these controls will vary based on a variety of factors from the complexity of proposed work to location of works.

### 4.3.1. Defining Terms

TERM	DEFINITION
<b>Development in the vicinity of Heritage Item or Heritage Conservation Areas (HCA)</b>	<p>For the purposes of defining 'vicinity' in this section, it is land adjoining or located within the visual catchment of a <b>heritage item</b> or <b>heritage conservation area (HCA)</b>.</p> <p>The visual catchment is dependent on the location of the proposed development in addition to the bulk and scale of the proposed new development.</p> <p>All development that is adjacent to or in the vicinity of a <b>heritage item</b> or <b>HCA</b> needs to address the likely effect the new development has on the heritage significance and setting. The potential impacts of the development on a heritage item or <b>HCA</b> and the measures to minimise this impact should be located within the <b>Statement of Environmental Effects</b>.</p>
<p>Buildings within the heritage conservation areas are categorised in to one of two categories. The <b>Contributory</b> and <b>Non-Contributory (Neutral) buildings</b> are identified within each Heritage Conservation Area Map in <i>DCP Section 4.5</i>.</p>	
<b>Contributory Buildings</b>	<p>provide good evidence of the main development period(s) and make a positive contribution to the character and/or heritage significance of <b>heritage conservation areas</b>. They have a collective significance and their retention is essential if the character of the area is to be maintained.</p>
<b>Non-Contributory (Neutral) Buildings</b>	<p>display qualities which do not add to the character of the <b>heritage conservation area</b>. They are not to be considered as a precedent for new work when assessing the merit of an application. <b>Non-contributory</b></p>

TERM	DEFINITION
	<b>buildings</b> may be demolished and replaced by new development sympathetic to the character of the <b>heritage conservation area (infill development)</b> .
<b>Infill Development</b>	New buildings within a <b>heritage conservation area</b> , referred to as an infill building, respect and are sensitive to its neighbours and should be in keeping with the street's established setbacks, scale, form and materials. In accordance with the Burra Charter principles, an infill building should however be clearly seen as a new building and not attempt to replicate original buildings or copy traditional detailing.

### 4.3.2. Design and Character

#### Objectives

- O1. To promote high quality design that complements the existing **streetscape character** and heritage significance of the **heritage item** or **heritage conservation area**.
- O2. To ensure that new developments will not adversely impact on the setting, **streetscape character** or views associated with any **heritage item** or **heritage conservation area**.
- O3. To ensure that additions or changes to the external appearance of **heritage items** and **contributory buildings** respect the original, built form, architectural style and character.

#### Controls

- 1) Development is to demonstrate how it respects the heritage values of the **heritage item** or the **heritage conservation area** through a **Heritage Impact Statement** as outlined in *DCP Section 4.2.3*.
- 2) Common elements and features of the **streetscape character** are to be identified in a streetscape analysis and incorporated into the design (e.g., view corridors, built form, fencing styles, extent of soft landscaping, significant trees and driveway locations).
- 3) New developments should be consistent with important horizontal lines of buildings in the streetscape, in particular ground floor levels and eaves lines, where appropriate.
- 4) Large blank area of brick or rendered walls should be avoided. Where this is not possible in the design, contrasting building materials and treatments should be used to break up the expanse of the wall.
- 5) Street elevations and visible side elevations should not be significantly changed. Additions should be located to the rear or to one side of the building to minimise impact on the **streetscape character**.

- 6) The design of any proposed additions or alterations should complement the existing building in its scale, form and detailing. However, it should be possible to distinguish the new work from the old, on close inspection, so that old and new are not confused or the boundaries/junctions blurred.
- 7) All new work and additions should respect the proportions of major elements of significant existing fabric including doors, windows, openings and verandas.
- 8) **Non-contributory buildings** may use contemporary design where it is sympathetic to the **heritage conservation area** and/or **heritage items** in the vicinity.

### 4.3.3. Scale and Form

#### *Objectives*

- O1. To ensure that alterations and additions to **heritage items** and **contributory items** are consistent with the scale and form of these items or buildings, and do not dominate or compete with the existing significant heritage fabric.
- O2. To ensure that the scale and form of development is consistent with the predominant scale and form of the **heritage conservation area**, and of adjacent **heritage items** or **contributory buildings**.

#### *Controls*

- 1) In streetscapes where development is of a consistent single storey height, upper floor additions are appropriate only if not readily visible from the street. However, ground floor rear addition remains the preferred option.
- 2) Attic style additions may be permissible, but there should be no visible alteration to the front of previously unaltered buildings. Front dormer windows are especially discouraged where a building itself is a **heritage item**, or part of a relatively unaltered semi-detached pair or row.
- 3) Dormer windows and skylights are not located facing street elevations or where they will be prominent from a public place or dominate the original roof form. The design of dormer windows should generally be appropriate to the style and scale of the building.
- 4) Additions should not visually dominate, compete with or conceal the original form and massing of the existing buildings.
- 5) Additions to **heritage items** should not contain any major or prominent design elements which compete with the architectural features or detailing of the existing building.
- 6) Where single storey rear additions are proposed to dwelling houses, the addition should not compromise the integrity of the main roof and is to be lower in scale and secondary to it.



- 7) Upper floor additions to the main roof of any single storey dwelling house may be acceptable if contained wholly within the existing roof space without change to the roof pitch or eaves height.
- 8) Upper floor additions to the rear of any single storey dwelling house should preferably use pavilion-type forms, with a lower scale linking structure between the original building and any double storey addition.
- 9) If a pavilion-type form is not suitable or desirable in the location, an upper floor addition may be acceptable, set well to the rear of the building to minimise impact on the main roof and to minimise streetscape visibility.
- 10) Where rear lanes exist, it may be possible to provide additional floor space in an outbuilding at the rear of the site, rather than as an upper-level addition to the dwelling itself.
- 11) Where rear additions are proposed to **semi-detached dwellings**, the additions should not compromise the symmetry and integrity of the front elevation or dominate the other house in the pair.
- 12) Where rear additions are proposed to **attached dwellings** (e.g., terrace houses), the additions should not compromise the integrity of the front elevation or the forms of relatively intact rear wings.
- 13) The scale of new **non-contributory infill buildings** should be compatible with the streetscape, (i.e., single storey, or single storey to the front with two storeys to the rear). The form should also be compatible, including roof form and articulation.

#### 4.3.4. Siting and Setbacks

##### *Objectives*

- O1. To conserve and maintain established setbacks to streets.
- O2. To ensure adequate **curtilage** and landscape setting for the building.
- O3. To ensure the integrity of the **heritage item** and its setting, or the **heritage conservation area**, is retained by the careful siting of new buildings and alterations and additions to existing buildings.

##### *Controls*

- 1) Development should conform to the predominant front setbacks in the street.
- 2) Development should respect side setbacks and rear alignments or setbacks of surrounding development.
- 3) Front and rear setbacks should be adequate to ensure the retention of the existing landscape character of the **heritage item** or **conservation area** and important landscape features.

- 4) Any significant historical pattern of subdivision and lot sizes should be retained. Subdivision or site amalgamation involving **heritage items** or **contributory buildings** should not compromise the setting or **curtilage** of buildings on or adjoining the site.

### 4.3.5. Detailing

#### *Objectives*

- O1. To ensure that original detailing is retained and kept in good repair.
- O2. To encourage the reinstatement of original elements and detail.
- O3. To ensure that alterations and additions and new development have a level of detail which is appropriate to the architectural character and style of the **heritage item** or **heritage conservation area** setting.
- O4. To ensure that the pattern of door and window openings is clearly related to the placement, proportions and scale of existing fenestration of the heritage fabric.

#### *Controls*

- 1) Only detailing which is known to have been original to a building is to be considered acceptable. Do not add what was never there.
- 2) Retain and repair original doors, windows, original sunhoods, awnings, gable detailing and other decorative elements to principal elevations. Original leadlight and coloured glass panes should be retained where possible especially where such is a significant element of the item.
- 3) Where original windows, doors and façade detailing have been removed and replaced with modern materials, consideration should be given to reconstructing original features.
- 4) Authentic reconstruction is encouraged. Decorative elements should not be introduced unless documentary or physical evidence indicates the decorative elements previously existed. Undertake thorough research before attempting to reconstruct lost detail and elements.
- 5) Alterations and additions should incorporate new doors and windows which are compatible with the position, size, and proportions and detailing of original windows and doors.
- 6) Alterations and additions should adopt a level of detailing which complements the heritage fabric and should (in general) be less elaborate than the original.
- 7) Decorative elements of **non-contributory buildings** should adopt a level of detailing which is less elaborate than original buildings and does not mimic inappropriate heritage detailing.

### 4.3.6. Materials, Finishes and Colour Schemes

#### *Objectives*

- O1. To ensure that the selection of materials and colours is informed by the original finishes and palettes match those used in the **heritage item** or **heritage conservation area**.
- O2. To ensure that the visual quality of the **heritage conservation area** is maintained and upgraded by encouraging the use of appropriate colour schemes in all development

#### *Controls*

- 1) Materials for pathways and driveways are consistent with the character of the **heritage item** or **heritage conservation area**.
- 2) Changes to materials (including roofs and walls) on elevations visible from a public place are discouraged. Original face brickwork should not generally be rendered, bagged or painted. The removal of external brickwork skin is not supported.
- 3) Matching materials should be used in repairing the fabric of external surfaces. In the case of new face brickwork, the colour and texture of the brick, the type of jointing and mortar colour should be carefully matched.
- 4) New or replacement roof materials should match existing materials. Alternative materials may be considered appropriate to the architectural style of the building and the streetscape context, and are to be submitted for approval.
- 5) Alterations and additions use materials and colours similar to, or compatible with, the original material or colours.
- 6) New development of **non-contributory buildings** should have regard to the original colour or significant colour schemes within the **heritage conservation area**.

### 4.3.7. Roofs and Chimneys

#### *Objectives*

- O1. To retain the characteristic roof forms of **heritage items** and **heritage conservation areas**.

#### *Controls*

- 1) Attic rooms are to be contained within roof forms and should not dominate the street and visible side elevations.
- 2) Roofs should not be re-pitched or have their eaves line raised to allow for the provision of attic rooms.
- 3) Chimneys should be retained.

### 4.3.8. Verandahs and Balconies

#### *Objectives*

- O1. To ensure the retention and encourage re-instatement of early verandah and balcony forms.
- O2. To ensure that alterations and additions do not detract from or reduce the importance of original verandahs and balconies.

#### *Controls*

- 1) Front verandahs and balconies are to be developed at a compatible scale to their existing **streetscape character** where these features are a characteristic feature of the **heritage conservation area**.
- 2) Original front verandahs and balconies should be retained and conserved. Consider opening up verandah enclosures or infills, to reinstate an original open verandah.
- 3) Infilling or enclosure of front verandahs and balconies is not supported.
- 4) Additional verandahs should not compete with the importance of the original and should be simple in design and based on existing detail or an understanding of appropriate designs for each period or style.

### 4.3.9. Garages, Carports, Car spaces and Driveways

#### *Objectives*

- O1. To minimise the visual impact of carparking on heritage streetscapes.
- O2. To ensure parking structures and paved areas are visually discreet and do not dominate or compete with original character buildings.

#### *Controls*

- 1) Existing rear lane access or side street access (where available) are utilised for carparking in preference to front access.
- 2) Carparking structures are to be located to the side, or preferably to the rear of the building. Garages and carports should be setback behind the building line.
- 3) Open hard stand car spaces may be provided forward of the building line, but are located adjacent to a side boundary, and generally not be greater than single car width.
- 4) Existing building fabric, including verandahs and balconies, should not be altered to allow for the provision of a carparking structure or an open stand car space.
- 5) Open hard stand car spaces should not dominate the setting of the building in terms of loss of planting, fencing or retaining walls.

- 6) Carparking structures are to be unobtrusive and adopt materials, form and details which harmonise with and do not obscure views of the building. They are not made larger by the provision of a bulky pitched roof.
- 7) Existing driveways constructed of two separate wheel strips contribute to the character of the streetscape and should be retained where possible.
- 8) Large areas of concrete should be avoided and alternative materials such as pavers, gravel or permeable paving should be considered.
- 9) Buildings housing original stables, coach houses and interwar motor garages should be retained and conserved wherever possible.

#### 4.3.10. Fences

##### *Objectives*

- O1. To encourage the retention, repair or reconstruction of original fencing.
- O2. To encourage fencing in character with original buildings.
- O3. To encourage side and rear boundary fencing which is consistent with height and materials of original fencing.
- O4. To encourage consistent fencing where there this is a significant element in the **heritage conservation area**.

##### *Controls*

- 1) New and replacement front fences should not obscure building facades. High solid front fences are not appropriate.
- 2) New fence heights and form should be appropriate to the character of the **heritage item**, or to the **heritage conservation area**.
- 3) Lych-gates should not be provided unless there is evidence that they originally existed.
- 4) Side fencing forward of the building line should be simple with a level of detail and of materials and height compatible with the **heritage item, contributory building or heritage conservation area**.
- 5) Side and rear boundary fences should be preferably of traditional timber construction or otherwise of masonry construction. Colorbond metal fences are generally not considered appropriate.
- 6) Fences forward of the building line are to be consistent in materials and detail with the period and/or style of the primary building
- 7) New fences are to be compatible with surrounding development.

### 4.3.11. Gardens, Garden Elements and Swimming Pools

#### *Objectives*

- O1. To retain or reinstate landscaped settings and elements (particularly pathway location and materials) for **heritage items** or buildings within the **heritage conservation areas**.
- O2. To provide appropriately landscaped front garden areas in keeping with those of the area's significant early houses, properties and streetscapes
- O3. To improve the streetscape setting of all buildings in the **heritage conservation areas**.

#### *Controls*

- 1) Significant trees and landscape elements such as pathways, garden beds and structures should be retained.
- 2) Large areas of hard paving are to be minimised.
- 3) Garden and ancillary structures are appropriate to primary buildings in terms of scale, style and materials.
- 4) Swimming pools should be located at the rear of the property and where possible should retain important trees and areas of soft landscaping. Swimming pools should not result in significant changes to ground levels on the site.

### 4.3.12. Access and Mobility

#### *Objectives*

- O1. To ensure that development to facilitate access and/or adaptable dwelling and universal housing provision does not adversely affect the heritage fabric of the **heritage item** or **heritage conservation area**.

#### *Controls*

- 1) Modifications and alterations to facilitate access and mobility are sympathetic to the heritage values and heritage fabric of the original building.
- 2) Alterations and additions to facility access and mobility should be designed to be reversible.
- 3) Preserve heritage items or heritage fabric of higher significance if a compromise is required.

### 4.3.13. Commercial Properties

#### Objectives

O1. To ensure that original characteristics of traditional local centre retail buildings are retained and enhanced.

#### Controls

- 1) Original forms, details, materials and finishes should be retained, including original shopfronts, original suspended awnings and open balconies at first floor level.
- 2) Where the property is part of a single larger building, changes to ground level shopfronts and upper-level facades should not detract from the integrity and group value.

### 4.3.14. Service and New Technologies

#### Objectives

O1. To minimise the prominence of new building services and technical equipment in heritage items.

#### Controls

- 1) Air exhaust or ventilation systems, skylights, air conditioning systems, solar energy panels, TV antennae and satellite dishes should not be visible on the main elevation of the building or attached to chimneys where they will be obvious. Services and equipment should be installed at the rear, within the roof space or flush with the roof cladding and at the same pitch. They are to be of modest size and not prominent from the street.
- 2) Essential changes to cater for electrical or telecommunications wiring, plumbing or other services should be limited to what is essential to permit the new use to proceed.
- 3) Rainwater tanks are to be located at the rear or side of the dwelling and suitably screened. They should not be obvious from the street.

## 4.4. Specific Controls - Heritage Conservation Areas

**Heritage Conservation Areas (HCAs)** are more than a collection of individual **heritage items**; they comprise areas in which the historical origins and relationships between various elements create a sense of place that is valued and worth preserving. **HCAs** are identified by analysing the special characteristics which make up its heritage significance. This may include its subdivision pattern, the consistency of building materials or architectural style, the common age of the building stock and /or its social connectivity to the origins of its locality.

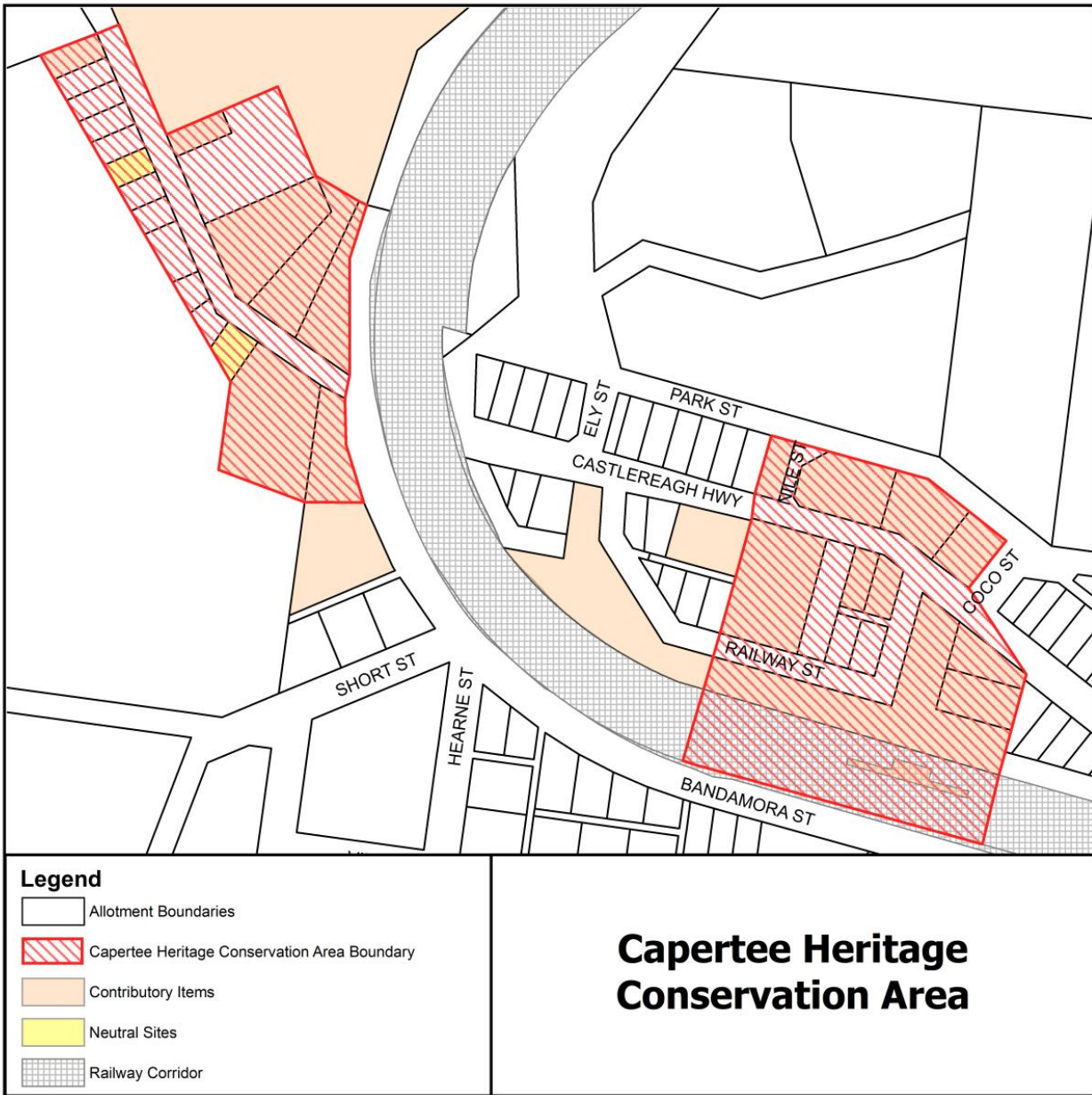
There are thirteen (13) **Heritage Conservation Areas (HCAs)** with individual attributes in the Lithgow Local Government Area as identified by the heritage provisions of the [LLEP2014](#). The controls in this section apply to the areas identified in Schedule 5 Part 2 of the [LLEP2014](#).

Further detail on each heritage conservation area, including photos, is included in the [Lithgow Heritage DCP Study](#) prepared by Paul Davies Pty Ltd, dated October 2010.

This section will prevail over any inconsistency between this Section and Section 4.3 in relation to the individual heritage conservation area objectives and controls.



### 4.4.1. Capertee Heritage Conservation Area “C1”



#### Period

- First occupied by settlers in the 1820s
- Hotel opened around 1870
- Construction of the railway in 1882

#### Area's Significance

Capertee was first occupied by settlers when John Jamison established a cattle ranch in the area known as 'Capita'. The area became known as Capertee Camp as it was a source of good water on the track to Mudgee. A hotel opened around 1870, a post office shortly after even though the village remained very small. A number of large cattle properties developed around the locality. The

construction of the railway to Capertee in 1882 established the area and the township by providing easy access to mining at Glen Davis (discovered in 1873), Torbane and surrounding areas.

The town has, however, had a mixed history since that time with a school, police station, court house and lock-up being established along with several inns and stores but relatively little other development. The remaining features includes two small churches, the railway station and several railway residences, the school and several residences, a community hall, the police and court group, the Capertee Hotel which has had an interesting history with numerous additions, several former early stores and former inns and a random collection of residential and former commercial buildings. The town is now served by one general store and service station and the hotel.

### ***Important Features***

The Capertee HCA is split in to two parts, reflecting the development and separation of the township. The town has a collection of important structures located along Capertee Highway which provides insight to the how Capertee developed. Such important heritage structures include the Police Station, Court House buildings, Catholic Church and the Civic Hall. It is also of interest that there appears to be significant areas of land within the town that are neither public road or lots of land, a large one is on the northern side of Railway Street.

### ***Objectives***

- O1. To ensure new developments are sympathetic to the heritage character of the Capertee Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape in protected and enhanced.
- O4. To promote the retention of original and characteristic shop front elements.

### ***Controls***

- 1) **Street Alignment and Setbacks:**
  - a) New buildings should adopt the setback of adjacent sites. Should there be a considerable variation in the frontal setback of adjacent structures, setbacks should be determined by the nature of the development (i.e., residential buildings set further back than commercial or civic buildings).
  - b) Buildings on large sites should not be built across the whole width of sites.
  - c) Buildings should be set in landscaped garden settings.
- 2) **Height, Form and Scale:** New buildings are to be no greater than one storey in height to reflect the modest scale of surrounding heritage buildings.

- 3) **Materials and Facades:** Non-painted surfaces are not be painted.
- 4) **Kerbing and Street Elements:** Existing road finishes should be maintained without kerbing and guttering.
- 5) **Outbuildings and Ancillary Structures:**
  - a) Outbuilding and ancillary structures are generally supported in the conservation area given that the following design principles are adhered to:
    - i) ancillary structures should not visually dominate sites from the public street;
    - ii) should be modest in form and use of materials;
    - iii) should be recessive and neutral in colour; and
    - iv) should use traditional elements related to the character of the property on which they are constructed such as matching roof pitches and use of materials.
  - b) The siting of ancillary buildings should be designed to minimise their visual impact on the precinct.
- 6) **Alterations and Additions:**
  - a) Alterations and additions structures are generally supported in the conservation area given that the following design principles are adhered to:
  - b) Additions should be generally at the rear of buildings and of similar scale and form to the main building;
  - c) Additions should be sympathetic to the existing buildings design but not necessarily matching; and
  - d) Where additions are visually prominent, they should not dominate the existing building or site, or impact adversely on the form of a significant building.
- 7) **Infill Development:**
  - a) There are a number of vacant lots within the conservation area that are capable of development. New infill development where appropriate should:
    - b) follow existing development patterns and setbacks
    - c) not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
    - d) not be dominated by garages and driveways;
    - e) integrate with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites; and

f) comply with other controls set out in this DCP.

8) **Landscape and Fencing:**

a) New developments should integrate the existing mature landscape and retain all significant plantings.

b) Where fences are added, they should be constructed from:

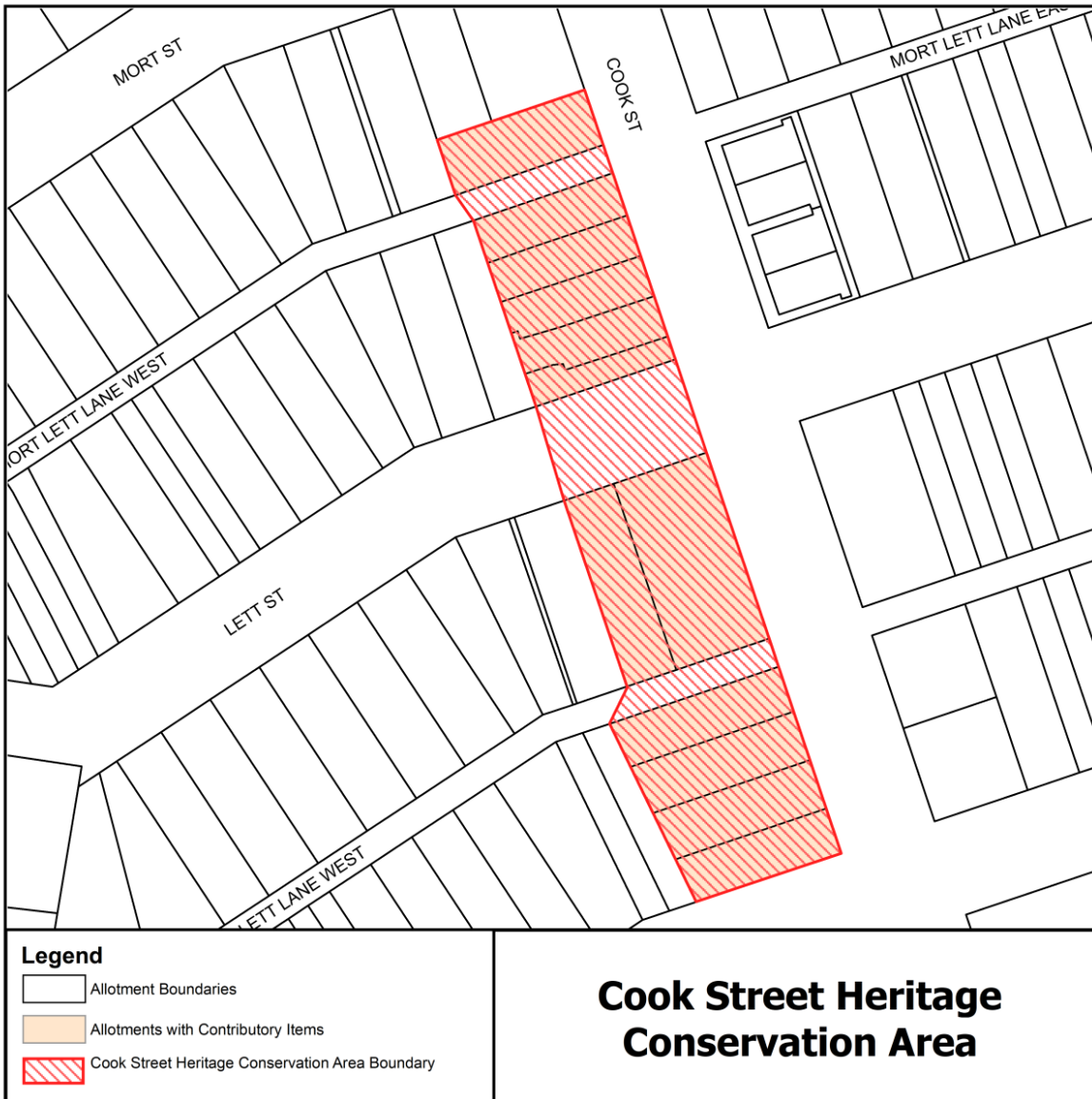
- i) simple timber picket style fences where that is consistent with the period of the house; or
- ii) simple timber framed fences with wire infills or similar modest fence styles.

c) Generally, brick or modern fencing is not appropriate.

d) Wire fencing or simple timber fencing are the preferred side fencing options for the precinct.

9) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.

**4.4.2. Cook Street Heritage Conservation Area “C2”**



**Period**

- Nos 28 - 40 Late Victorian (c1880)
- Nos 42 - 54 are an early twentieth Century

**Area’s Significance**

This is a small conservation area, located on the western side of Cook Street comprising two sets of speculative cottages, mostly co-joined, and stepping down the slope of the street. The area is visually connected to the edge of the Main Street Conservation Area extending the Main Street area to the south and linking it to the early residential development that types housing in inner Lithgow.

**Important Values**

Nos 28 - 40 are a continuous row of single storey semi-detached row housing with a high level of intactness and integrity. Several of the houses are abandoned and in poor condition requiring

securing and restoration. They feature a simple front verandah, small projecting bay to the front room, a simple roof gable detail over the front facade to each pair of houses and consistent facade treatment, roofs and detailing. Like many houses in the area, they are set quite close to the street with a small garden.

Nos 42 - 54 are face brick, have consistent brick fences, simple projecting bays with recessed front verandahs and simple window hoods. These structures are a group of detached houses also stepping down the slope of the street with no. 40 being an unusual third house on what are otherwise semi-detached residences. They retain a high level of intactness and integrity.

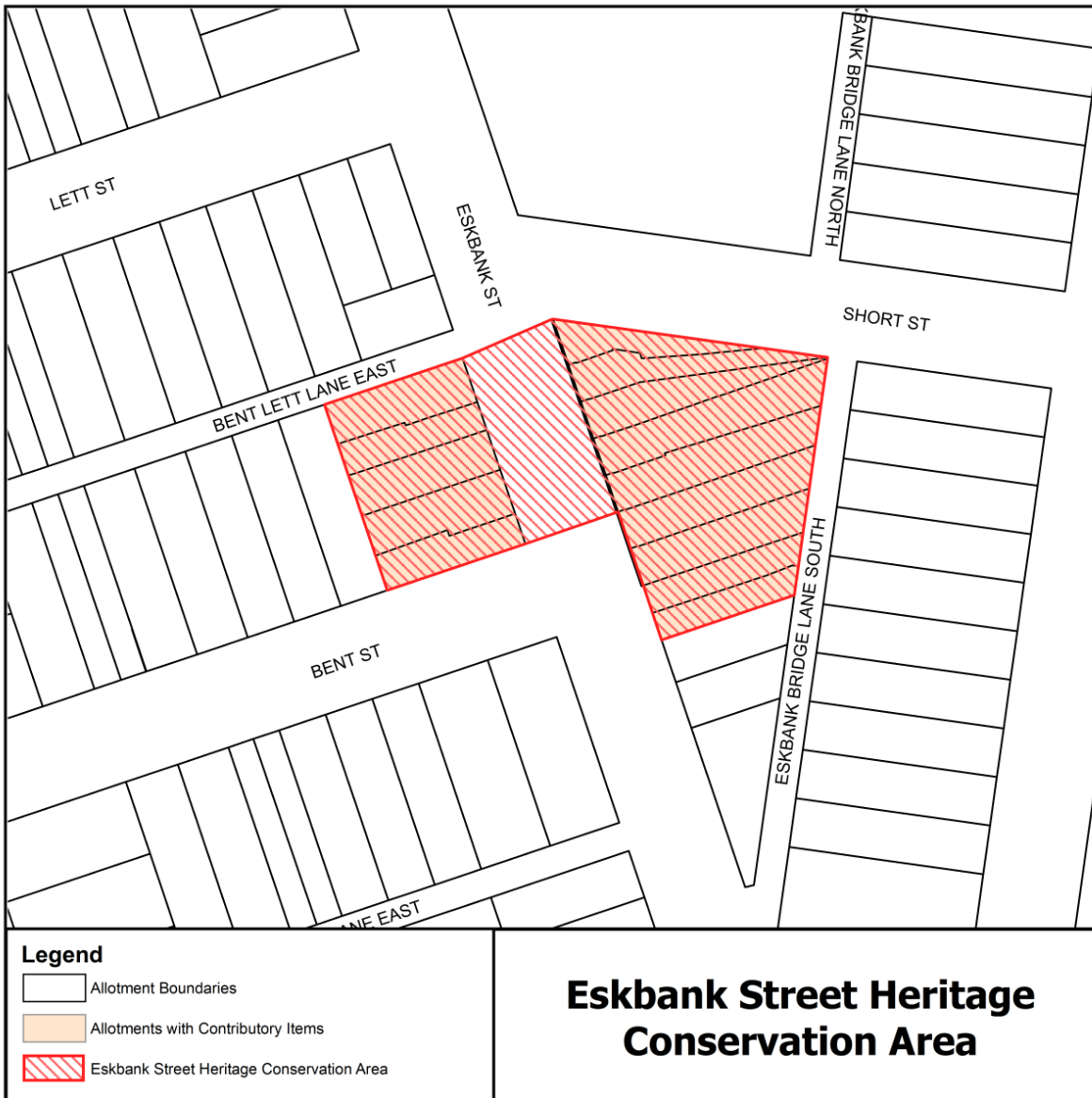
### **Objectives**

- O1. To ensure development is sympathetic to the heritage character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To ensure the mature frontages on Cook Street are maintained.

### **Controls**

- 1) **Height, Form and Scale:**
  - a) Buildings should be no greater than one storey in height.
  - b) Existing roof forms and configurations should be maintained.
- 2) **Kerbing and Street Elements:** The existing kerbing and street detail should be retained.
- 3) **Alterations and Additions:** Alterations and additions structures are generally supported in the conservation area given that the following design principles are adhered to:
  - a) Additions should be generally at the rear of buildings and of similar scale and form to the main building;
  - b) Additions should be sympathetic to the existing buildings design but not necessarily matching; and
  - c) Additions should not be visible from Cook Street
- 4) **Landscape and Fencing:**
  - a) Front gardens should be retained with no constraints on planting.
  - b) The existing brick fences should be retained.
- 5) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.

### 4.4.3. Eskbank Street Heritage Conservation Area “C3”



#### Period

- No 9 & 22 – 28 are early twentieth century
- Nos 11 - 23 are late Victorian

#### Area’s Significance

This is a small conservation area, located on the both sides of Eskbank Street comprising two groups of speculative cottages, mostly co-joined, stepping down the slope of the street. The area is visually connected to the edge of the Main Street Conservation Area extending the Main Street area to the south and linking it to the early residential development that types housing in inner Lithgow.

#### Important Values

Nos 11 - 23 are a continuous row of co-joined single storey row housing with a high level of intactness and integrity. These houses feature a simple front verandah, each with a small projecting

bay to the front facade, a simple roof gable detail over the front facade and consistent facade treatment, roofs and detailing. Like many houses in the area on the front boundary alignment. They have a consistent colour scheme.

No 9 at the end of the group is of good detailing that adds to the setting of the adjacent group. Immediately opposite is a small group of four houses, no's 22 - 28, that comprise a single house, a pair of semi-detached houses and the remaining half of a semi-detached pair, one of which has been demolished. They represent the later phase of speculative development of the city.

### **Objectives**

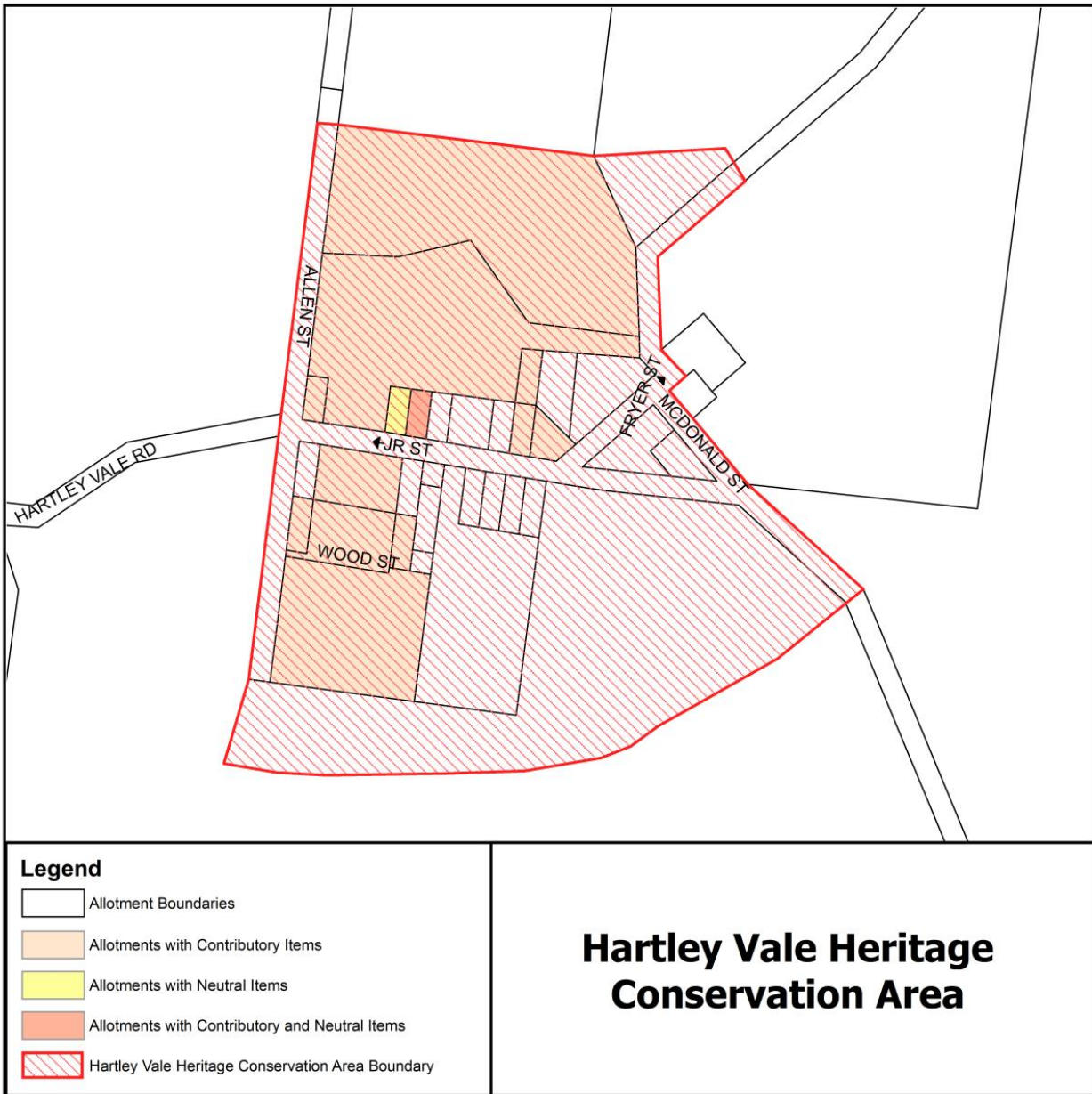
01. To ensure development is sympathetic to the heritage character of the Heritage Conservation Area
02. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
03. To ensure the visual amenity of the streetscape is protected and enhanced.
04. To ensure the mature frontages on to Eskbank Street are maintained.

### **Controls**

- 1) **Height, Form and Scale:** Buildings should be a maximum of one storey in height
- 2) **Kerbing and Street Elements:** The existing kerbing and street detail should be retained.
- 3) **Alterations and Additions:** Alterations and additions structures are generally supported in the conservation area given that the following design principles are adhered to:
  - a) Additions should be generally at the rear of buildings and of similar scale and form to the main building;
  - b) Additions should be sympathetic to the existing buildings design but not necessarily matching; and
  - c) Additions should not be visible from Eskbank Street.
- 4) **Infill Development:** New Infill Developments should follow the form and general shape of the demolished building with a matching built form to provide streetscape continuity.
- 5) **Landscape and Fencing:**
  - a) Front gardens, where they exist, should be retained with no constraints on planting.
  - b) The existing fences are to be retained.
- 6) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.



4.4.4. Hartley Vale Heritage Conservation Area “C4”



**Period**

Developed from 1860

**Area’s Significance**

Hartley Vale is a small rural village set in a rural valley with a handful of remnant early cottages, a former school and a hotel. The hotel is the centre of the village. The town is surrounded by rural lands on the floor of the valley and steep wooded hillsides. The location has very strong picturesque and visual quality that largely derives from its historic character set against the rugged natural surroundings.

The area developed from kerosene shale mines that promised to provide a new source of fuel of the colony. The Hartley Kerosene Oil and Paraffine Company developed a private township on 30 acres with a school and school residence. Initially there was no hotel and interestingly no church. The

Vale Hotel opened in the village in 1879 and a year later Comet Inn was opened (named after the kerosene brand). The town grew with more hotels, halls, shops, residences, a small department store etc. In 1913 the mining ceased, and the town dwindled to its present state where there is little apart from the hotel and school to indicate the level of activity and development in the vicinity.

### **Important Values**

Historically the village is of great interest through its mining connections and acts as a tourism anchor for the region. The character of the village now is very different from its historic character. The buildings are all found in a mature and established landscape setting, varying in different forms of condition. Collectively, they provide a nostalgic precinct that allows visitors to appreciate what was there and the semi-rural character of the buildings in the landscape.

### **Objectives**

- O1. To ensure development is sympathetic to the heritage character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To ensure that alterations, additions and the construction of new buildings and/or ancillary developments do not compromise the historical elements that contribute to the character of the area.
- O5. To retain the semi-rural character and open feel of the Hartley Vale Heritage Conservation Area

### **Controls**

- 1) **Street Alignment and Setbacks:**
  - a) New buildings if allowed should not be built to property boundaries.
  - b) A siting proposal should be required for any new building to ensure that the current visual quality of the town is not affected.
- 2) **Height, Form and Scale:**
  - a) Buildings should be a maximum of one storey in height
  - b) New buildings should be set above ground level (generally avoiding cutting and building off a ground slab).
  - c) New buildings should adopt traditional forms for roofs, elevational treatment and the use of verandahs.
- 3) **Facades:** Non-painted surfaces should not be painted.
- 4) **Fences:** Simple traditional rural fencing is the most appropriate style of fencing.
- 5) **Kerbing and Street Elements:**
  - a) The road and the current grassed verges should be retained.

b) The use of gravel for parking and edges is appropriate to the character of this HCA.

6) **Outbuildings and Ancillary Structures:**

a) Alterations and additions structures are generally supported in the conservation area given that the following design principles are adhered to:

- i) Ancillary structures should not visually dominate sites from the public street;
- ii) should be modest in form and use of materials;
- iii) should be recessive and neutral in colour; and
- iv) should use traditional elements related to the character of the property on which they are constructed such as matching roof pitches and use of materials.

b) The siting of ancillary buildings should be designed to minimise their visual impact on the precinct is a key to placing buildings in the precinct.

7) **Alterations and Additions:**

a) No alternations or additions are allowed on the following buildings:

- i) Small cottages;
- ii) The Company School.

b) Additions should be generally at the rear of buildings and of similar scale and form to the main building.

c) Additions do not need to mimic the existing building.

d) Where additions are visually prominent, they should not dominate the existing building or site or impact adversely on the form of a significant building.

8) **Infill Development:** There are a number of vacant lots within the conservation area that are capable of development. New infill development where appropriate should:

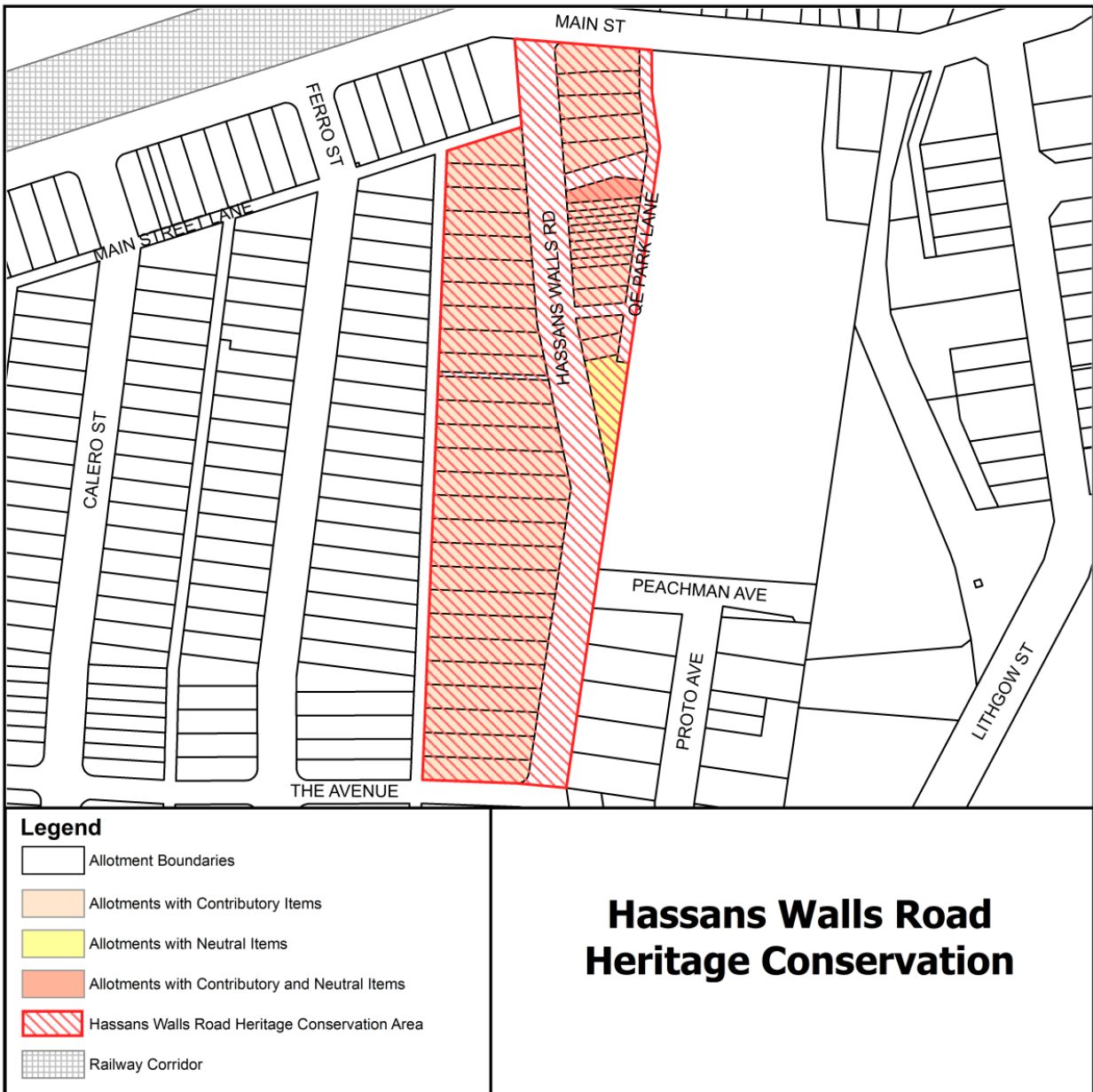
- a) follow existing development patterns and setbacks
- b) not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
- c) Not be dominated by garages and driveways, these should in all locations be to the rear and not seen from the main access road
- d) Integrate with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites
- e) Comply with other controls set out in this DCP.
- f) Involve minimal disruption to existing landforms.

9) **Landscape:**

a) The natural values of the area should be maintained as part of the historic character of the area.

b) The rural setting of the valley floor should also be retained with controlled new development only taking place.

4.4.5. Hassans Walls Road Heritage Conservation Area “C5”



**Period**

- Eastern side of the street is late Victorian period with early twentieth century infill.
- Western side of the street is part of the Extension Estate from 1908.

**Area’s Significance**

The conservation area in Hassans Walls Road abuts the edge of the Main Street Conservation Area and Queen Elizabeth Park. In many respects No. 1 Hassans Walls Road, which is a very fine house forms a gateway into the central area of Lithgow and marks the change from residential area to the city commercial centre.

In contrast to the other proposed conservation areas, this precinct contains a very fine collection of substantial houses as well as more modest houses from a range of periods but principally from the early twentieth century. This section of the street demonstrates the variation in class of housing in Lithgow related to location, aspect and setting.

### ***Important Values***

The area was developed in several stages in time. The eastern side of the street was developed in the late Victorian period with early twentieth century infill related to providing housing for the Small Arms Factory managers and foremen. The western side of the street was sub-divided as part of the Extension Estate in 1908. It is noted that the names of the eastern side housing are Scottish, linking them to the development of the Small Arms Factory.

The area also contains housing that is largely intact, or which has only minor changes and which establishes a very strong and intact streetscape. While there are many small groups and reasonably coherent streetscapes within Lithgow, there are relatively few areas where high levels of integrity and minimal change have occurred. In this precinct the overall quality and the slightly larger scale of the housing has led to the retention of most buildings in their built form. Of particular interest within the street are the small group of late Victorian two storey terraces (no's 14 - 159) and the buildings adjoining them also from the Victorian period (no's 137 - 141). These reflect the earlier development of this area with two of the buildings fronting the park. Also, of particular interest is the two-storey terrace style house at no 144, no 173 which is a very fine example of housing around the First World War and the eclectic no 134.

While the street contains new housing on the eastern side of the road to the south of the park, it has little impact on the overall quality of the precinct. There is no opportunity in the heritage conservation area for sub-division or infill development as the streetscape is complete, with the exception of no 163 which is the rear yard of the semi-detached building fronting the park. This lot should not be sub-divided or developed separately.

### ***Objectives***

- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.

## **Controls**

### **1) Street Alignment and Setbacks:**

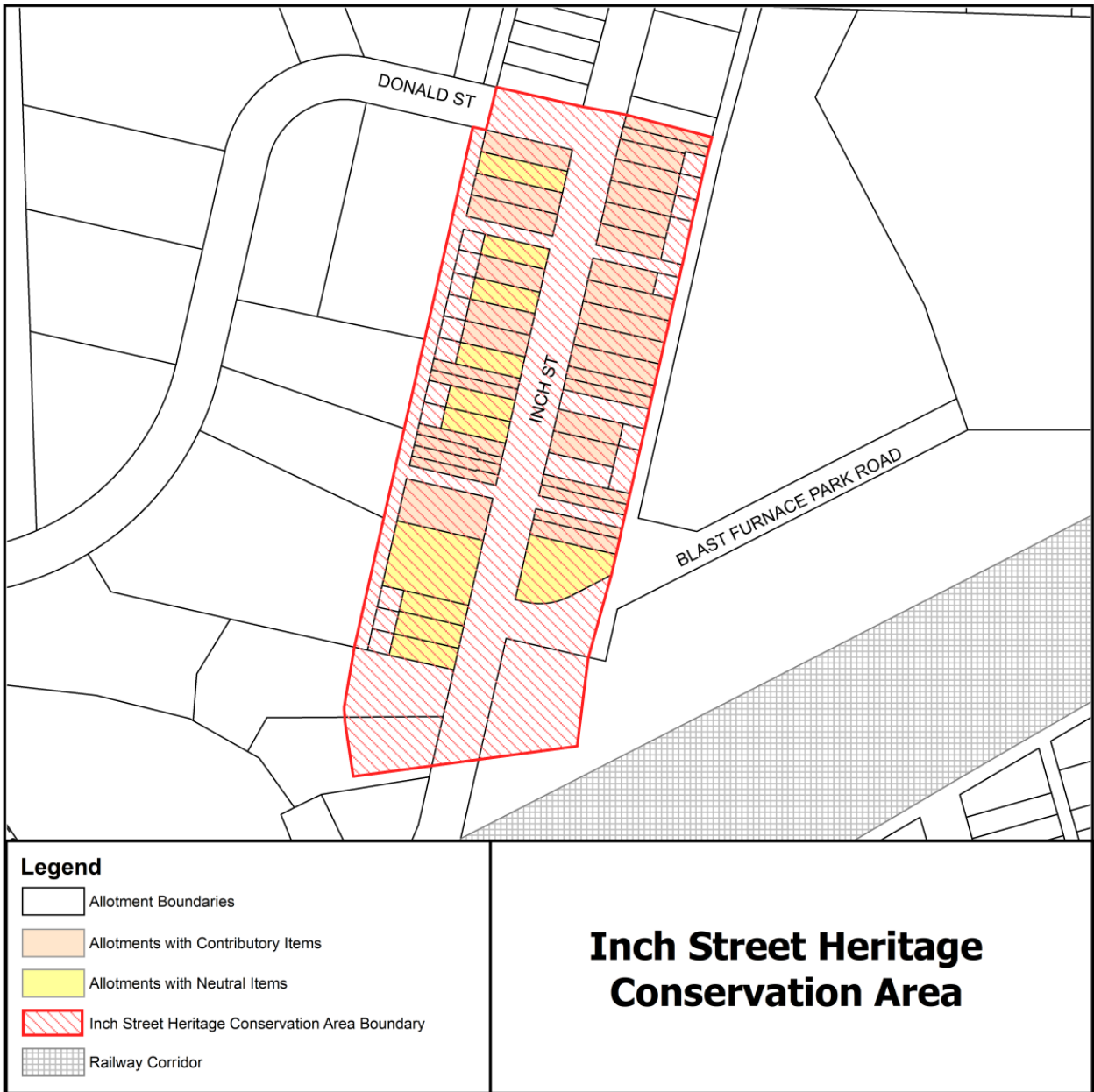
- a) There should be no change to the street alignments on any site.
- b) There should be no carports or garages on street frontages as all properties have rear access.
- c) There should be no change to the side alignments for the main section of the buildings.
- d) Rear additions should be designed to reflect standard setback requirements, except where they are highly visible from the street where additional controls may be required.

### **2) Height, Form and Scale:**

- a) Buildings are to be one storey in height (exception of the single two-storey building at 144 and several roof additions).
- b) Roof additions, second storeys, dormers and other forms of roof conversion that are seen from the street and which change the roof form of the house are not supported.

### **3) Facades:** Street facades should retain timber traditional joinery and traditional existing building detailing.

4.4.6. Inch Street Heritage Conservation Areas “C6”



**Period**

- Late Victorian for Nos: 1- 3, 9-21, 35-39 and 100-110
- Early twentieth century for Nos: 5-7, 25-29, 126-130, 136-138 and 142.
- Later twentieth century with buildings from the First World War to the present day for Nos: 31, 80-96, 112-116, 120-124, 132-134 and 140

**Area’s Significance**

This is a small conservation area, located on the northern side of the railway immediately adjacent to the now Blast furnace Park. The housing in the area is related to development of the Eskbank Estate with its industrial focus on the blast furnace and collieries.

### *Important Values*

The grouping is of high interest due to the variety of styles, periods and types of housing found in a small tightly defined area. While some buildings have been altered and a number date to mid twentieth or late twentieth century, they provide a cohesive precinct that demonstrates most characteristics of worker housing found across Lithgow in various locations.

There are three principal groups of structures, late Victorian; Early twentieth century and 142; and later twentieth century with buildings from the First World War to the present day. Unlike other conservation areas, this precinct contains a range of housing with a number of neutral buildings interspersed - most areas contain consistent groups of buildings often from a single or very defined development periods. While the neutral buildings are as a group altered in terms of detail and use of materials, they have all retained their external form and streetscape contribution as a grouping of early and mixed workers cottages.

Of particular interest is the fine, if sometimes modest, collection of Victorian buildings of very different scales, forms and styles. There are also a number of very good examples of later modest housing from the interwar and post war periods that give a layering of the historical development of Lithgow to the precinct.

While most sites are taken up there are several vacant lots and several larger lots that could accommodate further development. Recent Examples of infill housing suggest that care is required in the selection of design for new buildings so that they reinforce the character and value of the group.

The connection of the group to the railway bridge (now unused) and the blast furnace beyond gives the location an added significance as it forms a gateway to the Blast Furnace park area. Interpreting the housing, the industrial uses and the railway are key elements of the area.

### *Objectives*

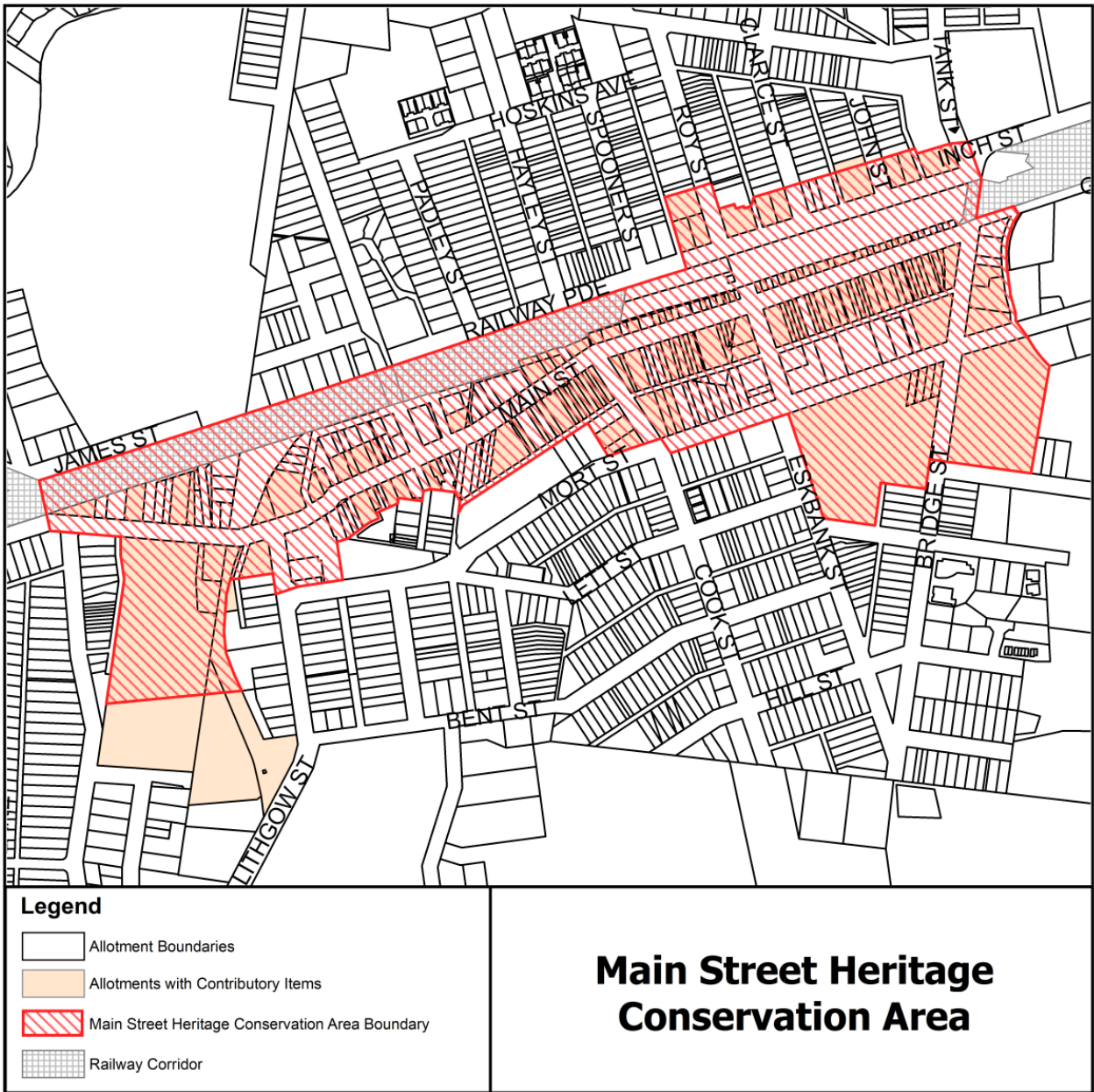
- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape in protected and enhanced.
- O4. To ensure that new infill developments enhance the character and value of the contributory items within the Heritage Conservation Area.
- O5. To protect visually important sightlines to the Railway bridge and the Lithgow Blast Furnace.



## Controls

- 1) **Street Alignment and Setbacks:** There should be no further infilling of verandahs or works to the frontage of existing buildings.
- 2) **Height, Form and Scale:** Infill development are to be limited to single storey and should use forms and scale elements that are found in the precinct such as steeper roof pitches, traditional roof forms and use of verandahs, etc.
- 3) **Fences:** Front fences are to be low, using timber, brick or traditional mesh wire.
- 4) **Infill Development:** There are a number of vacant lots within the conservation area that are capable of development. New infill development where appropriate should:
  - a) follow existing development patterns and setbacks
  - b) not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
  - c) have the principal facade oriented to the street
  - d) not be dominated by garages and driveways, they should be located to the rear on all lots
  - e) be single storey in form
  - f) provide consistent landscaping with the precinct
  - g) comply with other controls set out above
- 5) **Alterations and Additions:**
  - a) Additions should be generally at the rear of buildings and of similar scale and form to the main building.
  - b) Additions do not need to mimic the existing building.
  - c) Where additions are visually prominent, they should not dominate the existing building or site or impact adversely on the form of a significant building.
  - d) Where additions are located on the sides of the building, the new work should be designed in the style of the main building and at a scale that does not dominate the existing building form.
- 6) **Gardens:** Front gardens are to be retained where they exist.
- 7) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.
- 8) **Demolition:** Contributory buildings should not be demolished.

4.4.7. Lithgow Main Street Heritage Conservation Area “C7”



**Period**

- Late Victorian 1880-1900
- Edwardian 1900-1915
- The Twenties & Thirties 1915-1940

**Area’s Significance**

The Main Street HCA provides strong evidence growth of Lithgow from as far back as the mid 1800’s through its well-preserved shops, pubs and hotels which have been repurposed over time. The Main Street HCA encompasses the length of main street bounded by Bridge Street and Hassans Walls Road in addition to adjoining streets where there are visual, functional and use

connections including parts of Railway Parade, Cook Street, Mort Street and Eskbank Street. This section encompasses the majority of the Central Business District of Lithgow and includes the Cook Street Plaza area.

### ***Important Values***

The most significant feature of Lithgow's Main Street is the visual strength, unity and extensiveness of its historic architectural fabric which not only has considerable potential for physical restoration/reinstatement to its earlier lively character.

The HCA as a whole has retained a substantial number of its early buildings. The most important of buildings date from the boom years of Lithgow's development in the latter years of the 19<sup>th</sup> Century and into the early years of the 20<sup>th</sup>.

The street is lined by a strongly defined street-wall of relatively uniform height and alignment (generally of 2 storeys), particularly along the south side which gives it immediate identification as the main street.

The architectural character of the street has a notable harmony despite the variety of periods and styles, this being due to the fact that many buildings were erected in a relatively short period of times and the fact that even where of different periods their architectural characters share a number of important characteristics including masonry wall construction, use of parapets, sharing of a common repertoire of building forms and the use of vertically proportioned timber framed windows.

### ***Objectives***

- O1. To retain and where possible recover and enhance the heritage attributes of the precinct.
- O2. To ensure the consistency of building form and scale to add interest and rhythm to the street view.
- O3. To ensure that any infill development respects the consistency of built form, materials and detailing of the original shops in the precinct.
- O4. To ensure that new retail shopfronts are traditional in their form.
- O5. To locate the majority of parking outside of Main Street.
- O6. To encourage the reuse and adaption of the existing buildings.

## Controls

### 1) Street Alignment and Setbacks:

- a) Generally front setbacks should:
  - i) Occupy the full width of the block. Only vary from this where the specific locality has setbacks that are not on the street in which case new development should be considered in relation to both existing development that is adjacent and the overall character of the street in which it is located.
  - ii) Allow for street awnings or preferably street verandahs to new buildings and buildings that previously had verandahs and awnings.
- b) Generally, side setbacks should:
  - i) New infill development should extend across the frontage of blocks with zero side setbacks.
  - ii) Vehicle access should not be provided from main street frontages.
  - iii) In fringe areas of the precinct where setbacks vary, adopt adjacent setbacks as a guide to new development.
  - iv) Adopt zero lot alignment for lots with side frontages to lanes or minor streets.

### 2) Building Height:

- a) Existing patterns of height should be maintained: for example, if an area has consistent two-storey development, infill should be two storeys.
- b) Corner sites may be capable of slightly higher development but should not exceed the height of other development in the area.
- c) The overall pattern and rhythm of heights of development should be considered in determining infill development heights.

### 3) Existing Materials:

- a) Face brickwork should be retained as face brick and not painted.
- b) Unpainted render on buildings should not be painted
- c) Polychrome brickwork, patterned brickwork and facade tiling should be retained.
- d) Original shopfronts are rare and should without exception be conserved and retained.
- e) Existing joinery to facades should be retained and if it requires replacement should be replaced in matching joinery.

- 4) **New Materials:** New materials should relate to the existing range of materials in the locality. Common materials used are face brick, rendered facade elements, timber joinery, pressed metal soffits, patterned brickwork, metal and timber awnings and verandahs with painted finishes.
- 5) **Building Form:**
  - a) The form of any new building should relate to the context of the adjacent contributory buildings while maintaining its own integrity as a new element.
  - b) The overwhelming character of the precinct is relatively simple built forms, often with applied detailing depending on the style and period in which it was constructed. New work should retain a simple form approach with detailing that is consistent with contemporary building but using traditional material types and approaches to design.
- 6) **Roofs:**
  - a) The use of parapets is an overriding character of the precinct and is appropriate for new development, particularly in infill buildings in existing rows.
  - b) Elsewhere a range of roof types could be used but generally they should relate to the immediate context in type of roof, material, pitch and form. The shape of roofs is as important as detailing and material.
  - c) Very contemporary roof forms are not encouraged on street frontages, as they are uncharacteristic of the existing patterns of development.
  - d) Preferred roofing materials are corrugated iron, terra cotta tile or for key buildings or buildings that originally used it, slate. Large pan steel roofing should not be used where visible from the street.
- 7) **Facades:**
  - a) Original joinery, verandahs, etc should be retained or recovered. Non-painted surfaces should not be painted.
  - b) Infilled openings should be opened up with matching original joinery.
  - c) Verandahs should be recovered or reinstated.
  - d) Applied claddings should be removed, and the early facade form recovered where possible.
  - e) Use articulation of materials to create a predominant vertical pattern and rhythm to buildings.
  - f) Proportion openings to reinforce a vertical rather than horizontal proportion.
  - g) Use traditional built elements and forms such as dry pressed brickwork and modulate facades with devices such as engaged pilasters, recessed panels, shaped parapets, etc.

**8) Shopfronts and Interior Fitout:**

- a) Original shopfronts in intact or mostly intact form are to be retained, conserved and reinstated in any works. All early or original fabric should be retained and repaired and only replaced if repair is not feasible.
- b) The design of infill shopfronts to existing buildings where later shopfronts are being replaced should be generally to the pattern of original shopfronts, or use design elements from earlier shopfronts on the building such as:
  - i) recessed doorways, often with splayed walls
  - ii) solid plinths and lower panels
  - iii) solid framing to glazing
  - iv) the use of smaller glazed panels above the main glazing
- c) Designs can be adapted to allow for disabled or equitable access.

**9) Verandahs, Awnings and Balconies:**

- a) Existing awnings should be retained to provide shelter and protection unless they are to be replaced by reconstruction of earlier known verandahs or awnings.
- b) Key heritage buildings, such as corner buildings or visually prominent buildings that had street verandahs should as a priority have those verandahs reinstated. Associated work such as reinstating doors from upper floors should also take place.
- c) Replacement verandahs should be based on accurate research and design.
- d) New elements need to comply with structural codes that may require self-supporting structural elements.
- e) Awnings or verandahs on new infill buildings should be provided to development fronting commercial streets. The design should be compatible with the character of the new building and the setting.

**10) Fences:**

- a) Where fences are added they should be simple timber picket style fences where that is consistent with the period of the house or simple timber framed fences with wire infills or similar modest fence styles. Generally, brick or modern fencing is not appropriate.
- b) Side fencing varies but is generally wire fencing or simple timber fencing which are the preferred fencing types for the precinct.

**11) Kerbing and Street Elements:** The brick pavements have provided a high level of amenity and quality to the street and their use on adjacent streets over time is encouraged.

**12) Alterations and Additions:**

- a) Additions should be generally at the rear of buildings and can be of similar scale and form to the main building.
- b) Additions do not need to mimic the existing building.
- c) Where additions are visually prominent (perhaps due to the setting of a particular site) they should not dominate the existing building or site or impact adversely on the form of a significant building. The use of design techniques such as adding pavilions to an existing building that are lightly attached or a new wing to a building can be appropriate ways to undertake new work.
- d) No new work should be visible from the street frontage from a viewing position at head height on the opposite side of the road building frontage. This will ensure that the skyline of the street is not broken by large rear additions.
- e) On corner sites, rear additions need to be designed with a street frontage to the side street that relates to the pattern of development in that street. Side streets should be considered as main facades in addition to main street frontages for the purpose of designing new work.

**13) Infill Development:** There are a large number of vacant lots within the conservation area that are capable of development. New development is appropriate and should:

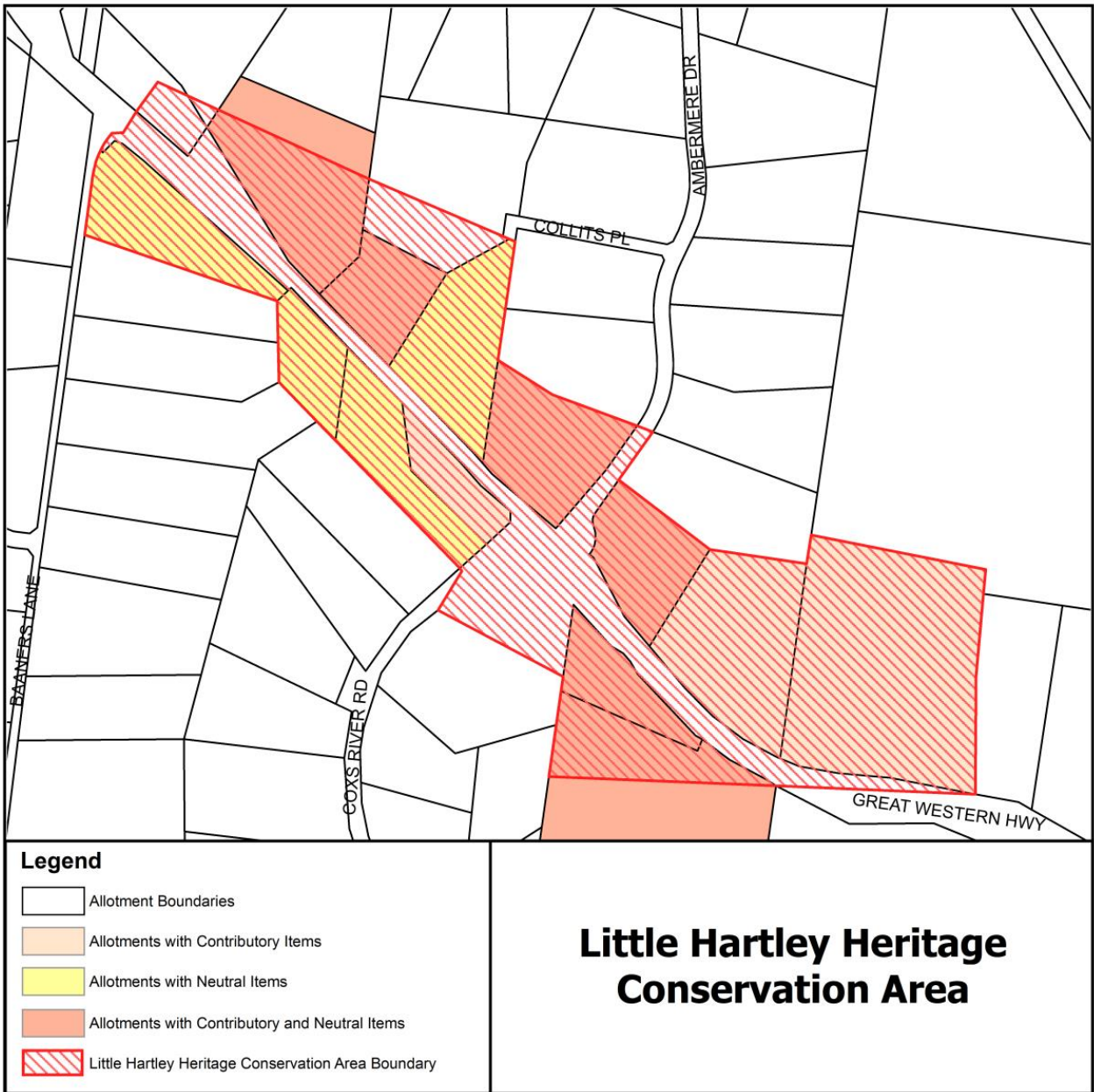
- a) follow existing development patterns
- b) not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
- c) not be dominated by garages and driveways
- d) integrate with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites
- e) comply with other controls set out above

**14) Landscape:**

- a) The landscape setting should be retained and enhanced.
- b) Street trees should be protected and incorporated into future plans.

**15) Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.

4.4.8. Little Hartley Heritage Conservation Area “C8”



**Area’s Significance**

Little Hartley is a tiny village located on the Main Western Highway. There are few early colonial roadside villages remaining in NSW although remnants of a number of early settlements remain. One of the inherent difficulties of understanding a location like Little Hartley is its linear and spread-out character without a core or centre.

**Important Values**

The location has an early history with a number of very early and fine colonial structures. They are spread out along the main road and it is hard to define a core area or focus for the village. The buildings comprise early inns, houses, farms and stores, one attached to the Harp of Erin was relocated from Hartley Vale when the mine closed, and the town went into decline.



Six of the key buildings are built close to the road and have a direct and important roadside relationship. Several are inns or shops and others are houses that have had a range of uses including accommodation that required direct road access. Other buildings are set back on larger land holdings as part of farms, they are quite difficult to see from the highway but still make an important visual contribution through their plantings, roof forms, entrances and drives, etc.

### **Objectives**

- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To protect the visually important rural landscapes and cultural plantings surrounding the historic houses and inns within the HCA.
- O5. To ensure new sub-divisions does not adversely affect the overall pattern of development.

### **Controls**

- 1) **Fences:**
  - a) Fences are to be of timber picket style design or timber framed fences with wire infills that is consistent with the associate period of erected structures on site.
  - b) Modern or brick fencing is not appropriate.
- 2) **Kerbing and Street Elements:**
  - a) New access points are to be done rural in character and kerb and guttering is not be introduced.
  - b) Local roads are to retain their low-key character, its grassed and gravelled verges and its edge plantings.
- 3) **Alterations and Additions:** Additions should be in the form of separate structures that are lightly attached or show sensitivity in their design when attached to an existing structure.
  - a) Infill Development: Where infill development is appropriate under broader planning controls, new development should:
    - b) Follow existing development patterns and setbacks
    - c) Do not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
    - d) New development is not be dominated by garages and driveways not be dominated by garages and driveways, they should be located to the rear on all lots

- e) Integrate new work with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites
  - f) Retain the open rural character of the road
  - g) Development should ideally cluster at the central areas of the precinct and not take place as ribbon development along the main road.
  - h) Signs should be restricted and limited to modest, non-illuminated signs that are consistent with the historic character of the precinct.
  - i) Comply with other controls set out above
- 4) **Landscape:** All new landscape elements are to be in character with the existing landscape features.
- 5) **Subdivision and Site Amalgamations:** Should a sub-division lead to new building entitlements located close to the main road, new buildings should not proceed or should be sited in a way to reduce visual impact of the road area unless the new building is complementary to the core village area.
- 6) **Demolition:** Contributory items should not be demolished.

4.4.9. Mort Street Conservation Area “C9”



**Period**

1920's

**Area's Significance**

This is a small precinct in two parts that encompasses an intact group of unusual houses built for railway employees in the 1920's. The Railway Commissioners were constantly looking for cost efficiencies and standardised, prefabricated housing (and other buildings such as station buildings and service structures) were developed, in this period using pre-cast concrete as drop in panels into a concrete framing system.

### *Important Values*

These two closely located streets form the largest and most intact grouping of such buildings in the State. They are of particular significance as part of the extensive railway infrastructure of Lithgow and are a very recognisable and tangible visual remnant of the system. Some of these structures were of considerable size, but all used the same modules and construction system.

The lots on which the buildings stand (they are all semi-detached houses built in pairs) are quite large and extend to rear lanes. The buildings have been constructed and set in a way to provide good side access between buildings. Overtime, a number of houses in the conservation area have included modest rear additions. The landscape treatment of each lot varies as does fencing, but all retain an overall character that is consistent with their significance.

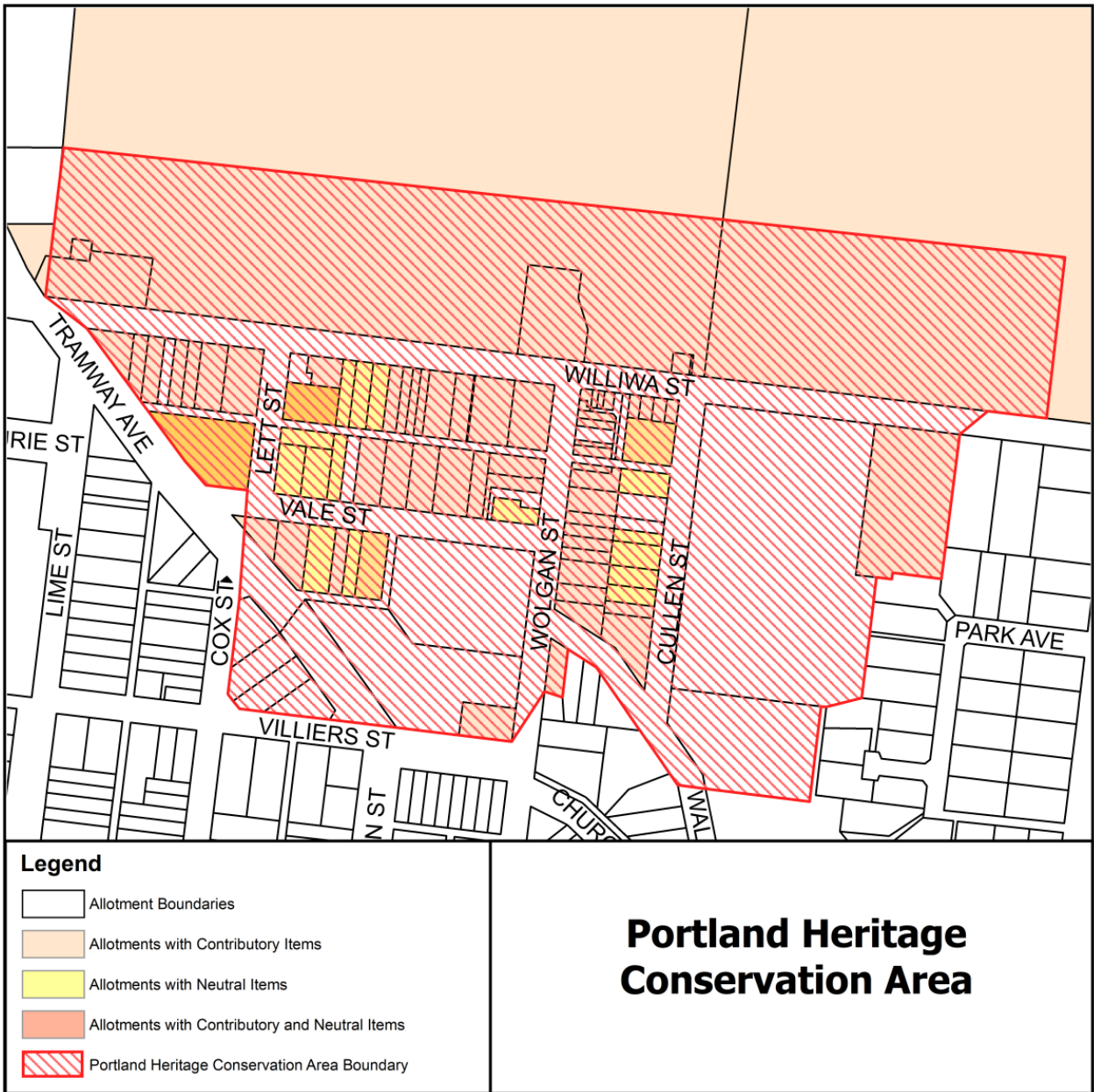
### *Objectives*

- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To promote the retention of original and characteristic shop front elements of each period.

### *Controls*

- 1) **Street Alignment and Setbacks:** Additions should not extend in front of the rear alignment of the main section of each building to maintain their streetscape presence and each building form.
- 2) **Height, Form and Scale:**
  - a) Additions, which are to be located to the rear of existing buildings should generally use the form of the existing building as a guide using similar roof slopes and materials.
  - b) Two storey additions are possible provided they do not overwhelm the main building.
- 3) **Materials:**
  - a) Materials for new work should be restrained and generally modest.
  - b) Sheet claddings and rendered brickwork would be suitable wall materials
  - c) Roofs should be constructed with corrugated iron.
- 4) **Fences:** Fencing should be low scale, timber or wire, but preferably not picket or deriving from earlier stylistic periods.
- 5) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.
- 6) **Demolition:** Contributory items should not be demolished.

4.4.10. Portland Heritage Conservation Area “C10”



**Period**

Started development in 1961

**Area’s Significance**

Portland developed as a town and location through the mining and burning of lime to form cement. Apart from being traversed by early settlers it was not until 1861 when Thomas Murray selected land and built a lime kiln (in the vicinity of Lime and Villiers Streets) that development took place.

Unlike many country towns, particularly those around Lithgow, the town dates from the early years of the twentieth century with very few earlier buildings and a relatively small number of later buildings. The quick development and high level of consistency and intactness in the buildings

reflects a quick development as the Cement Works reached their peak. The relatively low level of change over the last fifty years reflects the commercial decline of the town centre and the absence of development pressure. This has provided very contiguous and intact streetscapes, an overall character that is also consistent and due to the commercial success of the cement works quite substantial buildings for a small town.

### ***Important Values***

One of the very significant features of the town is the impact of the Cement Works on the form of the town. Not only do the works dominate the townscape as they extend along the whole of the northern side of Williwa Street, but the addition of community facilities such as the large community hall and the row of very fine early twentieth century housing creates a distinctive and unique streetscape directly opposite major commercial buildings.

The town centre also contains a very rare theatre and a co-operative store that provide clear evidence of the character and social cohesion of the township. Also, unlike most town centres in the Lithgow area, the town is compact, tightly defined and has almost no missing elements. It also has almost no intrusive elements, although several buildings have had superficial changes that would ideally be reversed to recover their earlier appearance.

Outstanding buildings within the town centre include: the hotels, the theatre, the co-operative store complex, the shops fronting Williwa Street, between Cullen Bullen Road and Wolgan Street with their very fine awnings and shopfronts (almost all remaining), the exceptionally fine shopfronts to no 39 Williwa Street, most of the commercial buildings fronting Wolgan Street, No 1 Wallerawang Road, the Swimming Centre, the Post Office and Police Station and the two church complexes.

The streetscapes themselves are also of high value, the rise in Williwa Street culminating in the crest near the service station provides a range of views in and out of the area and the relatively steep Wolgan Street creates a defined town centre. The recent addition of the small park in Wolgan Street that connects to the larger parklands to the east enhances the setting.

Elements such as street verandahs, intact shopfronts with a wide range of fine detailing, retention of traditional and early materials and a clear pride and care for the town present Portland as an exceptionally fine country township of very high heritage and civic value

### ***Objectives***

- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To promote the retention of original and characteristic shop front elements.

- O5. To preserve the view lines, particularly along Wolgan Street towards the Foundations site.
- O6. To ensure the appropriate re-use of the abandoned cottage buildings located on the northern side of Williwa Street.
- O7. To encourage appropriate in-fill development that reflects the existing commercial and residential built form.

### **Controls**

#### **1) Street Alignment and Setbacks:**

- a) New commercial buildings should generally have street awnings or verandahs that are consistent with the form of adjoining verandahs.
- b) Residential buildings should be set in landscaped garden settings.
- c) Commercial development on typical lots should be built boundary to boundary on the street frontage. Where rear or side access is available to sites for vehicles, vehicle access should not be available from the principal street frontage. Exceptions to this are sites such as garages where access already provided and essential for the function of the site.
- d) For residential development on the edges of the precinct statutory setbacks should apply, however on large sites building should not be built across the whole width of sites.

#### **2) Height, Form and Scale:** New buildings are to be no greater than two storeys in height.

#### **3) Infill Development:**

- a) New infill development should reflect the scale of surrounding buildings but generally should not exceed the typical scale in the vicinity.
- b) New elements should overall be recessive and restrained in their form and massing.

#### **4) Materials:**

- a) The use of materials is to be determined by the period of construction and the type of building.
- b) New materials should either reflect traditional use of materials (seen in rendered masonry and generally simple materials) and be modest and recessive in character. Materials such as modern extruded bricks, pre-coloured metal products etc are likely not to be appropriate.

#### **5) Facades:**

- a) Non-painted surfaces should not be painted.
- b) Existing tile facades, original metal shopfronts, decorative elements on facades and awnings should all be retained.
- c) Original materials, where they require replacement should be replaced like for like.

#### **6) Fences:**

- a) Where fences are added they should be simple timber picket style fences where that is consistent with the period of the house or simple timber framed fences with wire infills or similar modest fence styles. Generally, brick or modern fencing is not appropriate.
  - b) Side fencing varies but is generally wire fencing or simple timber fencing which are the preferred fencing types for the precinct.
  - c) Fencing will mostly be appropriate to residential development areas and not within the core commercial area.
- 7) **Kerbing and Street Elements:** The main roads are concrete kerbed and guttered, other streets are bitumen or gravel formation with kerbing and gutters. Existing road finishes should be maintained without changing the predominant concrete character of road surfaces.
- 8) **Abandoned Buildings:** The north side of Williwa Street, part of the Cement Works is fenced off and contains a large number of important but abandoned buildings. The Co-operative store is also unused and falling into severe disrepair. A strategy to find uses for and recover these buildings is very important to retain the very significant heritage character and future viability of the town. Where future uses are not imminent, temporary works should be undertaken to secure the structures, make them waterproof and prevent ongoing deterioration until such time as uses can be developed. This applies to outbuildings and minor elements of key sites as well as the principal buildings or features.
- 9) **Outbuildings and Ancillary Structures:**
- a) Where secondary or outbuildings are found on significant sites (for example the buildings behind the Co-operative store, behind 39 Williwa and 35-37 Williwa Street) they should be retained and conserved.
  - b) Where these structures are in poor condition, an assessment should be made to assess their potential prior to consideration of removal.
  - c) For new structures, ancillary structures should not visually dominate sites from the public street, should be modest in form and use of materials, should be recessive and neutral in colour and should use traditional elements related to the character of the particular property on which they are constructed such as matching roof pitches and use of materials.
  - d) Ancillary buildings are to be sited to minimise their visual impact on the precinct is a key to placing buildings in the precinct.
- 10) **Alterations and Additions:**
- a) Most buildings in the precinct are capable of adaptation with alterations and additions however, most such work should be at the rear of properties and should not affect public views and streetscapes.



- b) Additions should be generally at the rear of buildings and of similar scale and form to the main building (or of lesser scale).
- c) Additions do not need to mimic the existing building.
- d) Where additions are visually prominent (perhaps due to the setting of a site), they should not dominate the existing building or site or impact adversely on the form of a significant building.
- e) The use of design techniques such as adding pavilions to an existing building that are lightly attached or a new wing to a building can be appropriate ways to undertake new work.

11) **Infill Development:** There are several vacant lots within the conservation area that are capable of development. New development can be appropriate and should:

- a) Generally, follow existing development patterns
- b) For residential buildings (in particular) not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
- c) For commercial buildings in existing streetscapes:
  - i) Use awnings and verandahs of traditional form.
  - ii) Use traditional approaches to shopfronts though contemporary materials are acceptable.
  - iii) Use parapeted forms to the street.
  - iv) Use traditional forms and patterns of signs including hung under awning signs.
- d) Not be dominated by garages and driveways
- e) Integrate with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites

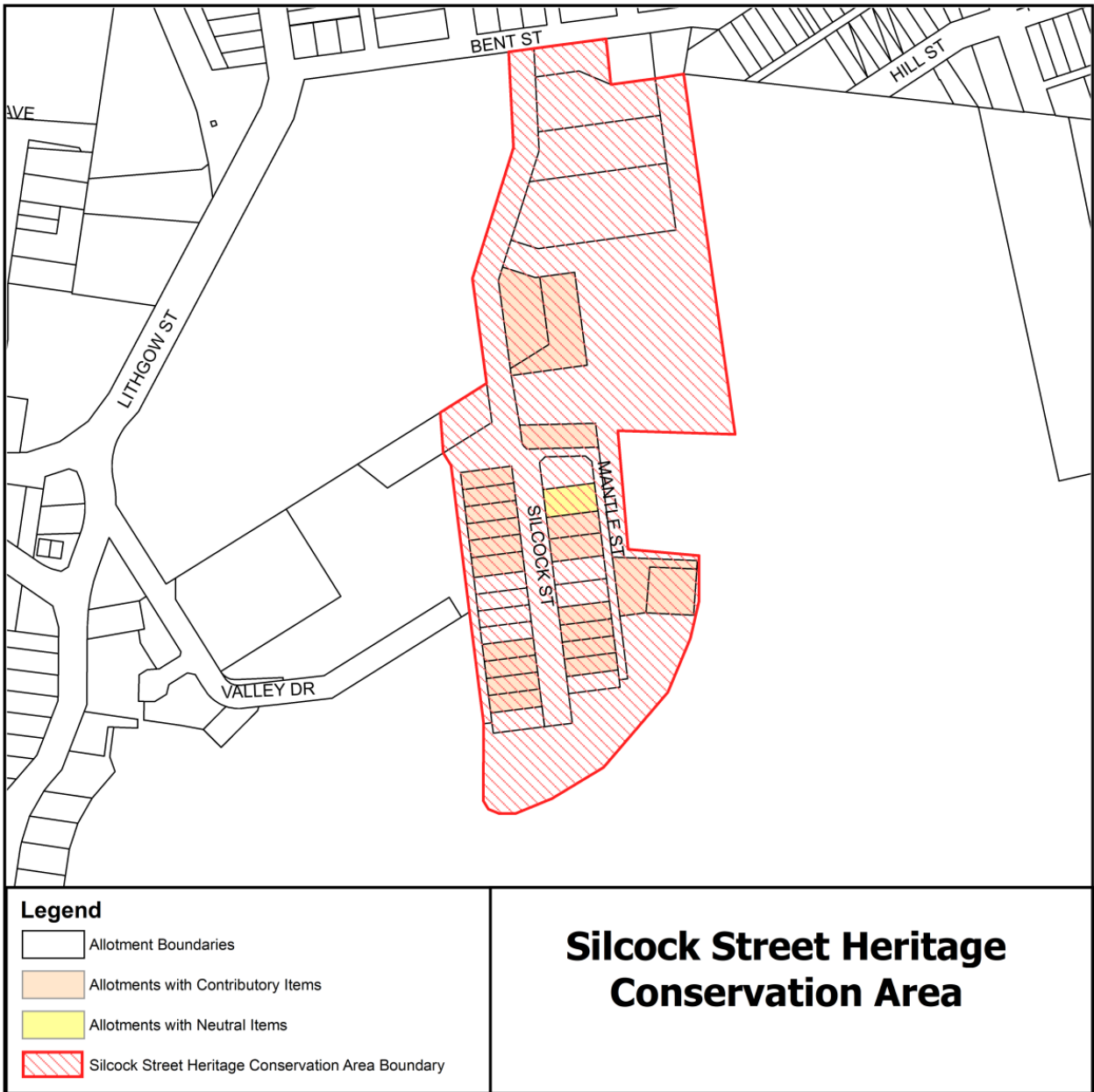
12) **Landscape:**

- a) The mature landscape should be retained on sites and in public areas and enhanced (as has taken place on numerous sites and public areas).
- b) New work should integrate the existing mature landscape and retain all significant plantings.

13) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area

14) **Demolition:** Contributory items should not be demolished.

4.4.11. Pottery Estate (Silcock Street) Heritage Conservation Area “C11”



**Period**

1878 to 1907

**Area’s Significance**

Silcock Street is a dead-end street that was once part of the Lithgow Pottery Works. The cottages have remained in company ownership, although vacant land is now being offered for sale and one new house has been added to the group. Lithgow Pottery Works commenced in 1878 and closed in 1907 although the brick and pipe works continued until 1946. The houses were built as part of the earlier pottery works and were occupied by company workers.

Remnant ruins and archaeological remains are located along the central section of the street with a group of houses built to accommodate pottery workers at the end of the street. The majority of houses are located facing each other across a wide street with two other earlier houses, one at the end and one behind the housing.

### *Important Values*

Most of the houses are of the same or very similar design with several being distinctly different. They clearly form a group and can be understood as a planned estate. Not every lot in the street was however developed when the estate was taken up and there are 7 vacant lots. One has been built on recently.

The houses are the most accessible and tangible remains of a major Lithgow industry that has considerable historic interest and value. The houses are able to collectively demonstrate approaches to industry with the provision of good quality worker housing close to the workplace. The buildings and their setting display a consistency in design and detail seen through to fences and kerbing - stone - that is not often seen in such groups.

It is also of interest that the estate is separated from other housing and was and continues to be surrounded by commercial and industrial development. The character of the estate, through its isolation will remain and a key element of its future management is the design of infill housing on the vacant lots. The first new house to be built follows the design of the existing housing and is a successful infill building.

### *Objectives*

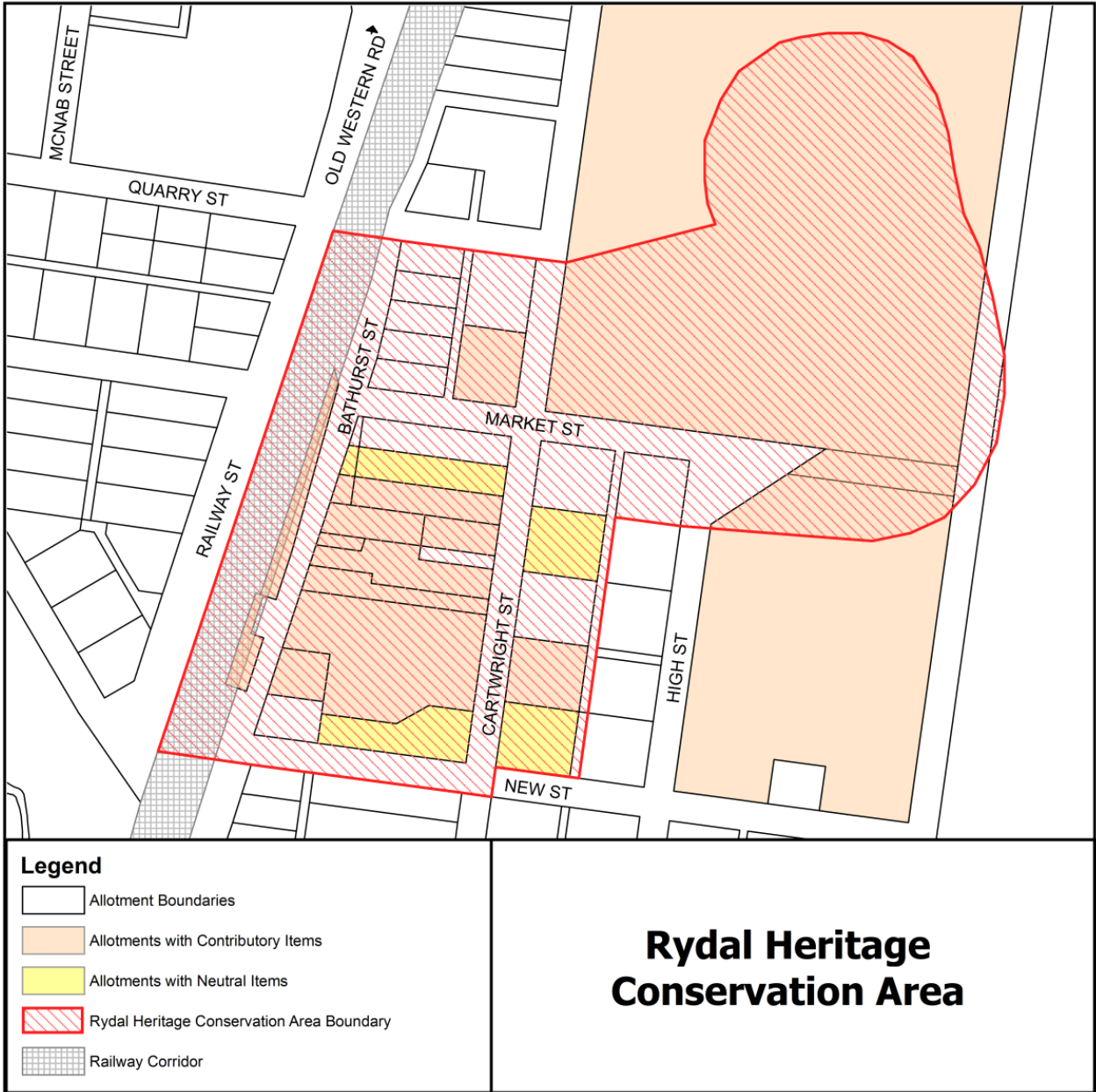
- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To encourage appropriate in-fill development and re-use that is sympathetic to the historical built form.
- O5. To retain the high level of consistency of façade treatments along Silcock Street.

### *Controls*

- 1) **Street alignment and setbacks:**
  - a) All access to properties should be from the rear of properties.
  - b) No front driveways are to be permitted in the area.

- 2) **Materials:** New developments are to use materials that are consistent with the traditional palette of materials used in the heritage cottages including face brick for facades, corrugated iron roofs, brick fences, traditionally proportioned joinery
- 3) **Facades:** New developments are to adopt the existing facade of the street.
- 4) **Fences:**
  - a) New fences should be made of brick done at a low scale to match the existing design of fencing.
  - b) Simple low fencing made of timber can be considered appropriate for the area, but not timber picket fencing.
- 5) **Kerbing and Street Elements:**
  - a) The road with is kerbed with stone and vergers are grass with a central sealed pavement which is to be retained.
  - b) Footpaths and new interventions are not to be made into public realm areas.
- 6) **Outbuildings and Ancillary Structures:** Where there is an opportunity to add outbuildings and/or garage, they are to be sited at the rear of the property and traditionally scaled and proportioned to adjacent structures.
- 7) **Alterations and Additions:** Additions are to be confined to the rear of the property, being no greater than one story in height and does not dominate the street view.
- 8) **Infill Development:** There are several vacant lots within the conservation area that are capable of development. New development can be appropriate and should:
  - a) Follow existing development patterns and setbacks
  - b) Not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings.
  - c) Have the principal facade oriented to the street.
  - d) Not be dominated by garages and driveways, they should be located to the rear on all lots
  - e) be single storey in form.
  - f) Provide consistent landscaping with the precinct.
  - g) Comply with other controls set out above.
- 9) **Subdivision and Site Amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area.
- 10) **Demolition:** Contributory items should not be demolished.

**4.4.12. Rydal Heritage Conservation Area “C12”**



**Period**

- The Western Railway station opened: 1870
- The township started in 1843

**Area’s Significance**

Rydal township is a small rural village centred on the main western railway. The railway and station opened in 1870. The township was named by Governor Gipps after Wordsworth’s hometown in England. The town was used to ship gold and produce to Sydney and an access point to Jenolan Caves using horse-drawn coaches. At its peak the town boasted eight hotels, five blacksmiths, a

school, police residence, two churches and five stores. Today one hotel and two churches remain in use and there are no operating commercial premises, the town having a population of around 80.

### *Important Values*

The core area of the town is formed by two blocks to the east of the railway with the showground on the rise of the hill behind. The major house of the town is located on the other rise behind dense vegetation and is not included in the conservation area, however it is separately listed as a heritage item.

Street fronts the railway and comprises the Hotel Alexander building, a fine structure that indicates the importance of the town in the past, a church, post office and a conference centre based around a major house. The railway station has been restored and is used as a civic building with well-established gardens and linear park. The street has mature plantings and several very fine individual buildings.

Street runs parallel to the railway a block removed and contains several houses the Anglican Church and mature landscape. The rear gardens of properties in Street extend to this street. To the east bushland and mature exotic trees provide a strong backdrop.

A number of buildings have been or are being conserved; this is evident in comparing current photos with those taken for the heritage study. Considerable civic improvement works to public areas has taken place and the town centre has a very fine appearance.

### *Objectives*

- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To encourage appropriate in-fill development and re-use that is sympathetic to the historical built form.

### *Controls*

- 1) **Street alignment and setbacks:**
  - a) Buildings should be set in landscaped garden settings.
  - b) Statutory setbacks should apply, however on large sites building should not be built across the whole width of sites.
- 2) **Height, form and scale:** Buildings are to be one or two storeys in height.
- 3) **Facades:**
  - a) Non-painted surfaces should not be painted.

- b) Original materials, where they require replacement should be replaced like for like.
- 4) Shopfronts and interior fit out: There are no requirements for interiors generally, however liturgical fit out of churches is of considerable significance and should be considered carefully if change is anticipated.
- 5) **Fences:**
- a) Where fences are added they should be simple timber picket style fences where that is consistent with the period of the house or simple timber framed fences with wire infills or similar modest fence styles. Generally, brick or modern fencing is not appropriate.
- b) Side fencing varies but is generally wire fencing or simple timber fencing which are the preferred fencing types for the precinct.
- 6) **Kerbing and street elements:**
- a) The main road is concrete kerbed and guttered, other streets are gravel formation without kerbing or gutters.
- b) Existing road finishes should be maintained without further kerbing and guttering.
- 7) **Outbuildings and ancillary structures:**
- a) Ancillary structures should not visually dominate sites from the public street, should be modest in form and use of materials, should be recessive and neutral in colour and should use traditional elements related to the character of the particular property on which they are constructed such as matching roof pitches and use of materials.
- b) Siting of ancillary buildings to minimise their visual impact on the precinct is a key to placing buildings in the precinct.
- 8) **Alterations and Additions:**
- a) Most buildings in the precinct are capable of adaptation with alterations and additions, the exception being the small timber church in Bathurst Street.
- b) Additions should be generally at the rear of buildings and of similar scale and form to the main building.
- c) Additions do not need to mimic the existing building.
- d) Where additions are visually prominent (perhaps due to the setting of a particular site) they should not dominate the existing building or site or impact adversely on the form of a significant building.
- e) The use of design techniques such as adding pavilions to an existing building that are lightly attached or a new wing to a building can be appropriate ways to undertake new work.
- 9) **Infill Development:** There are a large number of vacant lots within the conservation area that are capable of development. New development is appropriate and should:

- a) Follow existing development patterns
- b) Not cut and fill to bench sites but rather set floor levels above existing ground level as seen in existing buildings
- c) Not be dominated by garages and driveways
- d) Integrate with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites
- e) Comply with other controls set out above

10) **Landscape:**

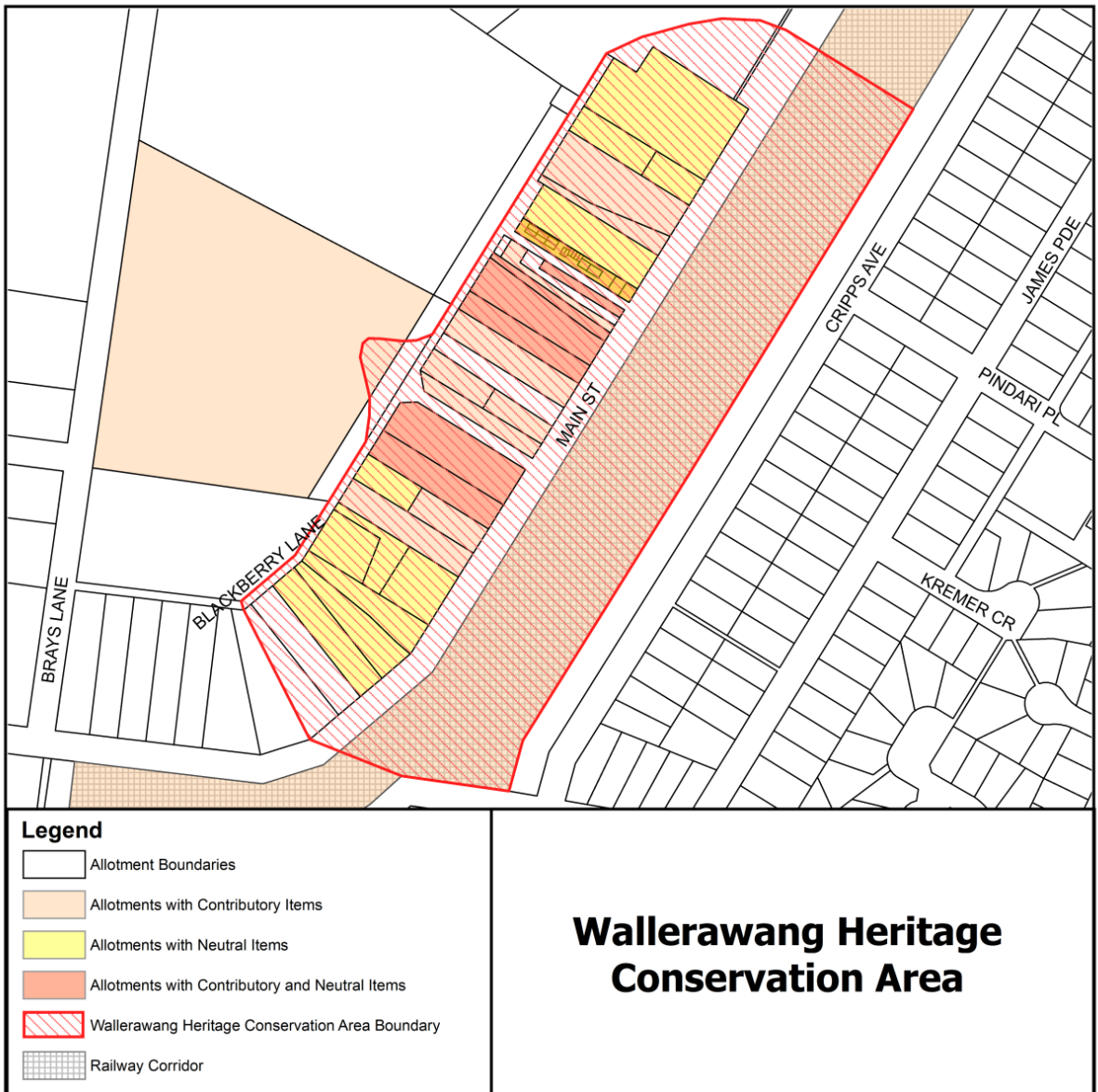
- a) The mature landscape should be retained on all sites and public areas and enhanced (as has taken place on numerous sites and public areas).
- b) New work should integrate the existing mature landscape and retain all significant plantings.

11) **Subdivision and site amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area

12) **Demolition:** Contributory items should not be demolished.



**4.4.13. Wallerawang Heritage Conservation Area “C13”**



**Period**

- 1860 the first school opened
- Railway line first arrived in Wallerawang in 1871

**Area’s Significance**

Wallerawang is an early railway town that was a key location for many years as both a junction and a point of connection to the goldfields and the surrounding rural areas. The scale and finesse of the railway station and associated buildings and the former extent of the rail yards indicates the importance of the town.

**Important Values**

The town centre reflects the importance of the railway with its single main street with buildings fronting the road and facing the railway complex. While the town has changed in its focus to power

generation and the railway has diminished in importance the town centre with the railway complex remaining one of the important railway town centres in the state.

Also, of importance in the conservation area is the range, integrity and number of heritage structures found along the Main Street. Even structures such as the former council chambers (now library and civic centre) and the childcare clinic are important elements of the history of the town and its development. Within the core town area there are only a few buildings that are neutral or that do not have heritage value fronting the street. Located on several of the deep lots (extending to Blackberry Lane) are found a number of contributory buildings and features that add to the group. Interestingly the town centre is single depth (noting several buildings on side streets) with the sports ground and rural land behind. The balance of the town is on the far side of the railway and to the west leaving the town centre relatively isolated. Other very significant early buildings are found some distance to the east reflecting the spread nature of the early town, these include the very fine St Johns church and the early school buildings.

Also, of interest is the range of building types and forms in the main street ranging from the railway station group, to the hotel, the post office, general stores and some housing.

Civic improvement works along the alignment of the railway station and the recent restoration of the station building have enhanced the area and create a focus for a conservation area.

While a number of buildings have been identified as potential heritage items, the railway group is outstanding, and the listing should include all of the extant elements, features and buildings from the junction and its signal box to the goods shed, crane and station group including the water tank, footbridge and signal box. The precinct has some landscape elements of value including the tree plantings in Daintree Lane leading to the war memorial and several fine mature trees in the park area.

### **Objectives**

- O1. To ensure development is sympathetic to the character of the Heritage Conservation Area
- O2. To ensure that the design and materials used in the new buildings, restorations and renovation of existing buildings is appropriate in scale and form to the character of the street.
- O3. To ensure the visual amenity of the streetscape is protected and enhanced.
- O4. To encourage appropriate in-fill development and re-use that is sympathetic to the historical built form.

### **Controls**

#### **1) Street alignment and setbacks:**

- a) Buildings are either built to, or close to the property alignment with the exceptions of houses, civic buildings and buildings along the side streets.

- b) New buildings are to be built to the street alignment, should have awnings and should be designed with a strong street presence.
  - c) Newer buildings at the western edge of the town are to be set back further than other buildings, breaking down the streetscape value of the group at that location.
  - d) An important visual aspect of the town is the ability to see through to the rural lands beyond and into gardens at several locations along the street.
  - e) Generally, infill buildings should respond to their immediate setting and the nature of the building. Commercial buildings should be built to the street alignment but necessarily side alignments.
  - f) Existing long-distance views should be retained to the north.
- 2) **Height, form and scale:**
- a) Buildings should be one or two storeys in height.
  - b) Awnings are a key feature of a number of buildings and provide part of the core heritage character of the town.
  - c) New commercial buildings should incorporate awnings over the footpath,
  - d) Buildings should fit into the overall pattern and rhythm of the streetscape.
- 3) **Materials:** Face brick, corrugated iron, areas of render, shopfronts that are articulated all reflect traditional character.
- 4) **Facades:** Non-painted surfaces should not be painted.
- 5) **Shopfronts and interior fit out:** Original shopfronts and interiors should generally be retained and where upgrade is required it should be integrated to retain the significant features of the building.
- 6) **Fences:**
- a) There are a small number of fences, predominantly picket fences with some timber/steel and wire infill fences.
  - b) Where fences are added they should be simple style fences where that is consistent with the period of the building or simple timber framed fences with wire infills or similar modest fence styles. Generally, brick or modern fencing is not appropriate.
  - c) Side fencing varies but is generally wire fencing or simple timber fencing which are the preferred fencing types for the precinct.
- 7) **Kerbing and street elements:** The main road is concrete kerbed and guttered with a concrete footpath, minor streets have grassed verges. Existing road and footpath finishes should be maintained.

**8) Outbuildings and ancillary structures:**

- a) Ancillary structures should not visually dominate sites from public streets, should be modest in form and use of materials, should be recessive and neutral in colour and should use traditional elements related to the character of the particular property on which they are constructed such as matching roof pitches and use of materials.
- b) New constructions are to use of complimentary forms and materials that reinforce and enhance the overall value of the area.
- c) Siting of ancillary buildings to minimise their visual impact on the precinct is a key to placing buildings in the precinct.

**9) Alterations and Additions:**

- a) Additions should be generally at the rear of buildings and of similar scale and form to the main building, although with larger buildings additions are likely to be more modest in scale.
- b) Additions do not need to mimic the existing building.
- c) Where additions are visually prominent (perhaps due to the setting of a particular site) they should not dominate the existing building or site or impact adversely on the form of a significant building.
- d) The use of design techniques such as adding pavilions to an existing building that are lightly attached or a new wing to a building can be appropriate ways to undertake new work.

**10) Infill Development:** There are a number of vacant lots within the conservation area that are capable of development. New development is appropriate and should:

- a) Follow existing development patterns
- b) Add to the activation and vibrancy of the commercial centre
- c) Not be dominated by driveways from Main Street
- d) Integrate with existing landscape where it exists and add new landscape that is consistent with the character of the precinct on open sites
- e) Comply with other controls set out above

**11) Landscape:**

- a) Mature landscape elements as seen in tree plantings should be retained and enhanced with additional plantings.
- b) Garden setting should be retained around houses and the park should add plantings over time.

**12) Subdivision and site amalgamations:** New subdivisions or boundary alterations are not to obscure or degrade the established subdivision pattern and character or the streetscape of the heritage conservation area**13) Demolition:** Contributory items should not be demolished.